

**DAVIS STRAITS
RESET STUDY**

FINAL REPORT

MAY 2017



CAPE COD
COMMISSION

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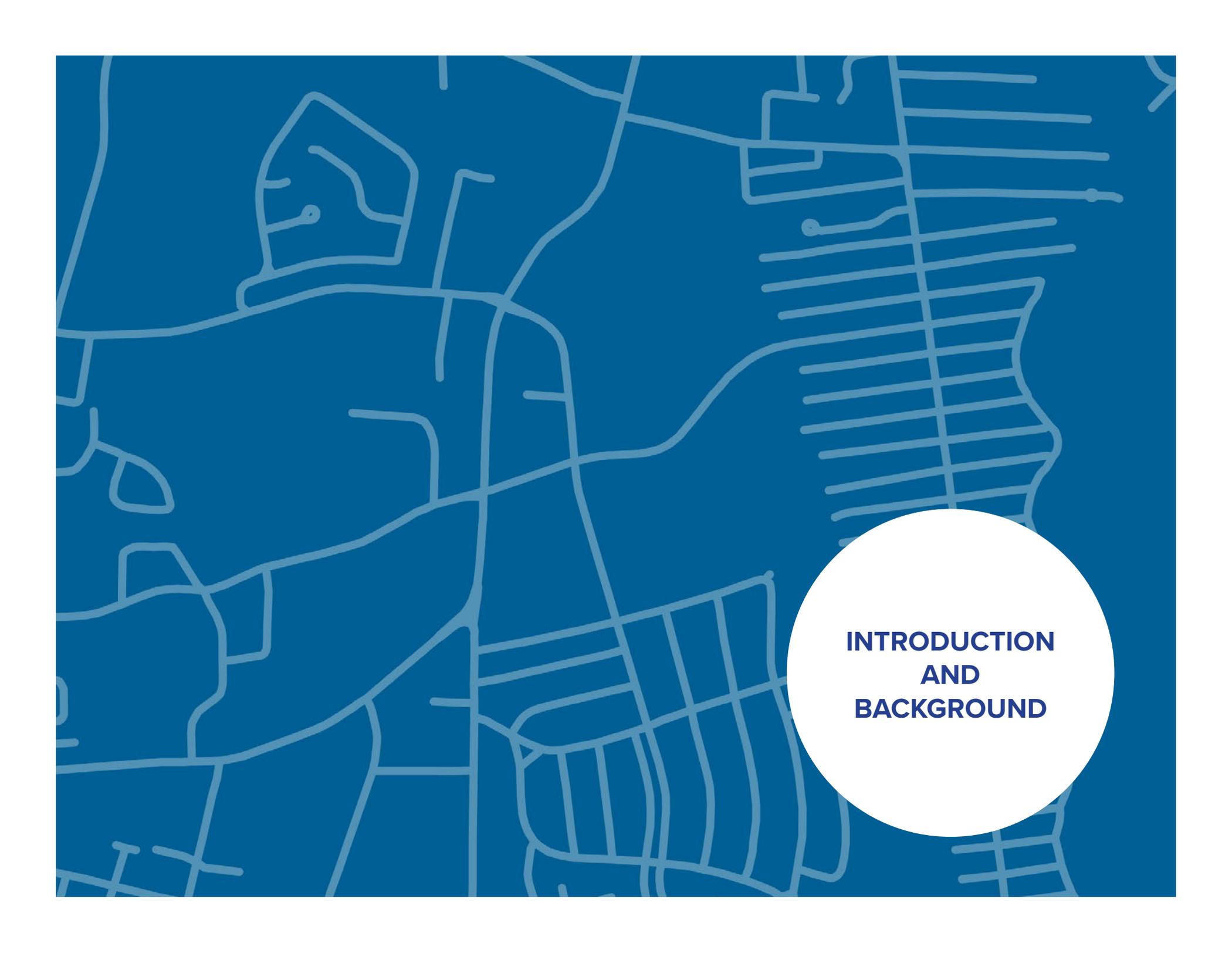
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**INTRODUCTION
AND
BACKGROUND**

The Town of Falmouth (Town) requested assistance from the Cape Cod Commission (Commission) to conduct a planning analysis of the Davis Straits/ Route 28 corridor between Scranton Avenue and Jones Road. This section of Route 28 is unique because it retains both an area of residential scale buildings as well as commercial plazas that have significant redevelopment potential. The area has experienced an increase in redevelopment interest in recent years and is believed to be ripe for change.

Currently, zoning regulations do not treat this area differently from other corridor commercial areas in Falmouth. The Town would like to differentiate this segment of Route 28 from other commercial areas, giving it a unique identity, and to improve community vitality and economic development outside of the downtown area. This project builds off the work of the Spring Bars Road planning study conducted by the Commission in 2013 (final report issued October 2013). The Falmouth Planning Board and Board of Selectmen have chosen to pursue several of the recommendations from that study.

STUDY AREA

The study area, outlined in Figure 1, includes the B2 zoning district located around Route 28 between Jones Road/Worcester Court and Falmouth Heights Road. It extends to a small section of the Business Redevelopment District (BRD) located between Falmouth Heights Road and Scranton Avenue.



FIGURE 1. STUDY AREA

PROJECT OBJECTIVES AND GOALS

The Town requested that the Commission provide technical assistance in two major areas:

1. Create a redevelopment plan for Davis Straits and Worcester Court, focusing on the stretch between Jones Road and Scranton Avenue; and
2. Develop plans for a Greenway Corridor linking Little Pond with Teaticket Park.

Commission staff conducted a planning analysis and developed recommendations to encourage economic vitality and a unique, pedestrian friendly character in the study area, highlighting access to town-owned facilities, which can act as a catalyst for redevelopment. Graphics, including a conceptual plan for the study area, are provided to support and illustrate the analysis and recommendations.

Project Goals

- Identify unique features of the area and ways to differentiate character from surrounding areas.
- Develop a recommended land use plan for street fronting properties along Davis Straits/ Route 28, acknowledging unique measures for small lots with individual properties and for larger commercial plazas within the study area.
- Recommend implementation tools, including zoning changes and other efforts, to protect the unique characteristics of the area and to facilitate development and redevelopment consistent with Town goals.

LOCAL COMPREHENSIVE PLAN

The Town of Falmouth adopted the following Land Use Community Goal at its November 2014 Town Meeting:

Land Use Goal: Falmouth shall use land appropriately to create places to **live, work, and play** by encouraging **sustainable**, and **balanced** growth.

The Town also adopted several land use policies to meet this overall Land Use Goal as follows:

- Policy #1: Falmouth shall continuously analyze the zoning rules that guide development to ensure that they are flexible, fair, and innovative to improve the quality of the public streetscape.
- Policy #2: Falmouth shall thrive and grow by guiding development into convenient and pleasant villages suitable for the 21st century, balanced with large reserves of open land.
- Policy #3: Falmouth shall encourage a high-quality and durable built environment that respects the culture of the community.
- Policy #4: Falmouth shall provide for a comprehensive network of access corridors, to accommodate many modes of travel.
- Policy #5: Falmouth shall encourage increased density, primarily through redevelopment, in some areas, and discouraging further development in others.

Also part of the Land Use element are several Action Items for Implementation that are consistent with the goals and objectives for the Davis Straits planning study, including the following:

- Initiate land planning to control flooding
- Adopt design review guidelines to be administered by the Planning Board
- Create a multi-family district zoning bylaw
- Hire a consultant to review areas of Falmouth where form-based zoning would be applicable, reporting on what benefits would accrue
- Acquire contiguous areas of Town as resource areas, which also have some potential compatible uses such as recreation, agriculture, and flood control

HISTORY

Davis Straits has changed significantly in the last 200 years. Reviewing the history that led to its character today can be useful in guiding decisions about its future. The area now known as Davis Straits was largely farmland until the mid-1900s. Before then, the roadway was lined with open fields and a few farmhouses, several owned by the Davis family. In the late 1800s, the stretch of road between the Emma Davis farmhouse at Falmouth Heights Road and the Andrew Davis home further east became known as 'Davis Straits'. Old photographs of the area show the road edged with fences to set off the fields, pasture lands and farmhouses. The Emma Davis farm comprised both pasture land and a dairy farm. Other agricultural products in the area were strawberries, turnips and cranberries. The principal cranberry bog in Teaticket was the Anderson bog accessed off Randolph Street.

Coastal resort development in the late 1800s brought increased traffic through the Davis Straits area, and some residents responded by operating small stores, farm stands, or offering overnight accommodations. Summer resort development was further fueled by the 1906-1908 transformation of the nearby Bowman's Pond/Deacon's Pond into Falmouth's inner harbor, which was accomplished by cutting a channel through to Vineyard Sound. By the early 1900s, some of the farmland was changing; a row of residential homes was built north of Spring Bars Road but farm fields continued to be worked behind the homes.

Another factor that influenced development in this area was locating the United Service Organization (USO) building roughly across from Scranton Avenue in 1941. This was one of the first USO buildings in the country and it was constructed to serve the troops stationed at Camp Edwards during World War II. It introduced many servicemen to the town of Falmouth and likely influenced many to stay. After the war, this building became Falmouth's Gus Cauty Community Center. It was more recently replaced by a new building located further back on the lot.

The northern reaches of Falmouth's inner harbor remained shallow mudflats bordering on pasture and farmland until 1953, when the town bought and dredged the area to create Davis Park and a boat landing. Development quickly filled in the land around it. The nearby marina opened in 1959.

In the 1960s, most of the remaining farmland along Davis Straits was sold for development, creating the Falmouth Plaza and Worcester Court businesses on the east side and smaller commercial developments on the west. A few residences remained north of Dillingham Ave until the 1970s, when they were replaced by additional small commercial developments. Also in the 1970s, the Anderson bog was filled in to become part of the Falmouth Mall. Upland areas around the bog then developed as commercial uses along Worcester Court and Spring Bars Road.

Today, Davis Straits contains development from numerous eras. Zoning requirements that guide the form of that development are discussed later in this report.





FIGURE 2. DAVIS STRAITS AND THE SURROUNDING AREA OVER TIME



1971



2014



**EXISTING
CONDITIONS**

VISUAL CHARACTER

The study area is defined by several different physical characteristics and uses. The northern part of the study area (see the green area in Figure 3) is characterized by smaller commercial buildings, often with a residential scale, and visible parking lots. The buildings may have multiple uses including residences on the second story. Landscaping is generally minimal, with few street trees.

The northeast section of Davis Straits (yellow area), in some contrast, includes smaller residential-scaled buildings, often with multiple uses. Parking is located to the side or rear, and landscaping is minimal. Most stakeholders have noted the scale and character of these residential buildings as desirable elements to retain and emulate elsewhere within the district.

The southern part of the study area (red area) is dominated by the Falmouth Plaza with a large parking area in front. These buildings have little distinctive character and are of more recent construction. Across Davis Straits from the plaza is an area of individual small scale buildings with large parking lots in front (blue area), resulting in buildings set further back from the road. While sidewalks are available on both sides of the road throughout Davis Straits, there are few pedestrian amenities, and very little buffer from the busy roadway.

The southeastern section of the study area (purple area) is characterized by light industrial uses and building types. These buildings are set back from the road with visible parking areas, some in front, and tend to be more functional in design. Buildings in the Business Redevelopment District, in the southwest corner of the study area (orange area), tend to be smaller commercial buildings of modern design; flat roofs, concrete walls, and few windows. While sidewalks are present in these areas, there are few pedestrian amenities or buffers to the roadway that offer safe passage through parking lots.

There is little to inform the driver or pedestrian that they have “arrived” in the Davis Straits area other than a clock at the intersection of Jones Road and Route 28. Additionally, there are no signs or indication that several public amenities are nearby, including Falmouth Inner Harbor, Teaticket Park, or the Little Pond conservation area.

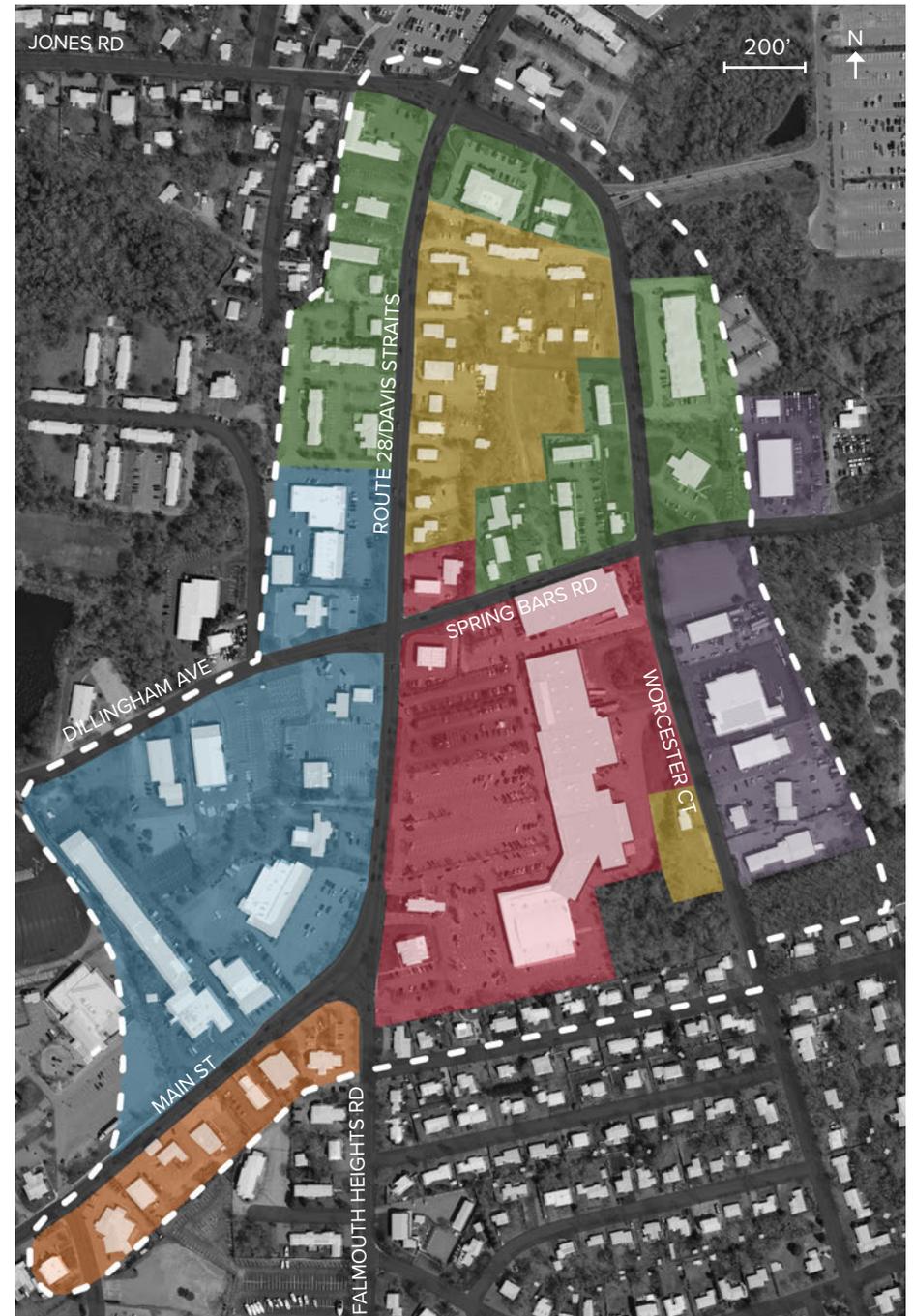


FIGURE 3. VISUAL CHARACTER ZONES IN THE STUDY AREA

TRANSPORTATION

Roadways in the Study Area

1. Davis Straits (Route 28)

Davis Straits (Route 28) serves as both an important corridor for regional travel as well as access to local businesses. Traffic volumes on Davis Straits (Route 28) range from 14,000 to 16,000 daily trips during average months to 18,500 to 20,500 daily trips during peak months. In both functions, the design of the roadway prioritizes the automobile, often to the detriment of bicyclists and pedestrians. Gaps in the sidewalk network, frequent and often wide curb cuts, and no dedicated accommodation for bicyclists all present challenges to non-motorists. The form of many of the adjacent land uses reinforces the predominance of the automobile along this corridor. Many businesses are set back from the roadway with parking lots in front. Very few businesses have safe and convenient connections between the sidewalk network and the businesses. Davis Straits (Route 28) is owned and maintained by the Massachusetts Department of Transportation (MassDOT).

2. Main Street (Route 28)

The character of Route 28 begins to change at the intersection of Falmouth Heights Road where, traveling east to west, it changes name from Davis Straits to Main Street. The portion of Main Street (Route 28) between Falmouth Heights Road and Scranton Avenue begins the transition into the Downtown with sidewalks on both sides of the roadway and the introduction of civic uses. Main Street (Route 28) is owned and maintained by the Town of Falmouth.

3. Worcester Court and Spring Bars Road

Worcester Court and Spring Bars Road serve a mix of commercial and residential uses. Traffic volumes range from 4,000 to 7,500 daily trips during average months and 6,000 to 9,500 daily trips during peak months. Given the lower vehicle volumes and more consistent sidewalk network than on Davis Straits, these roads are generally more pedestrian friendly than Davis Straits (Route 28). Both Worcester Court and Spring Bars Road are owned and maintained by the Town of Falmouth.

4. Davis Straits (Route 28) at Jones Road/Worcester Court

This signalized intersection is currently being reconstructed as part of a MassDOT improvement project. The project includes traffic signal upgrades,

minor geometric improvements, sidewalk reconstruction, minor drainage improvements, new pavement markings and new signage. The changes will improve safety and the accommodation of pedestrians and bicyclists.

5. Davis Straits (Route 28) at Dillingham Avenue/Spring Bars Road

At this unsignalized intersection, Dillingham Avenue and Spring Bars Road are stop-controlled while Route 28 remains free flowing. This intersection experiences significant queuing on Dillingham Avenue and Spring Bars Road presents a hazard to all road users. Despite some recent sidewalk improvements, the intersection can still be challenging to navigate as a pedestrian or a bicyclist.

6. Davis Straits (Route 28) at Falmouth Heights Road

While not as problematic in terms of safety or congestion as the intersection of Dillingham Avenue and Spring Bars Road, the intersection with Falmouth Heights Road presents its own unique challenges. The unusual configuration of this unsignalized T-intersection can lead to driver confusion. It also currently does not live up to its potential as a gateway to the Main Street area. This potential could be better realized with the conversion of the intersection to a roundabout as is currently supported by the Board Selectmen.

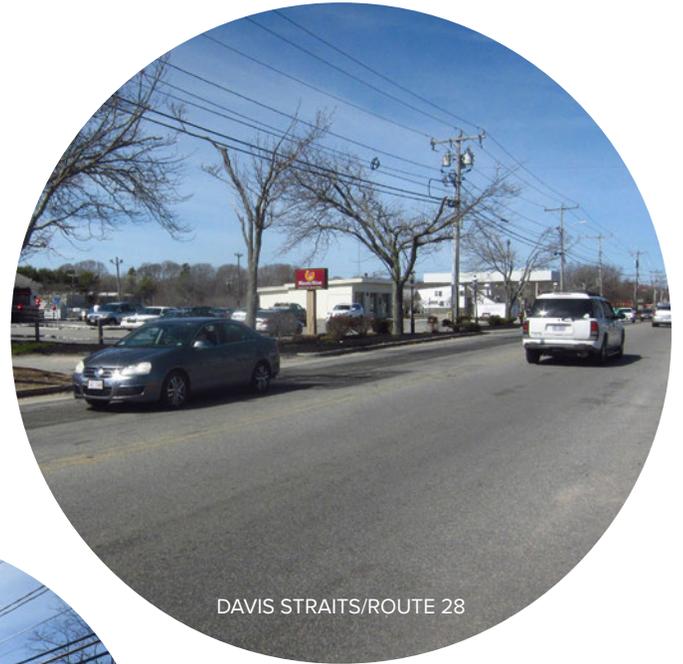
7. Main Street (Route 28) at Scranton Avenue

This unsignalized intersection experiences significant fluctuations in vehicle and pedestrian volumes in response to Island Queen arrivals and departures and activities at the Gus Canty Community Center, Fuller Field, the Police Station, and at the Falmouth Harbor. Vehicle queuing on the Scranton Avenue approach can be significant.

Additional Transportation Analysis

A detailed discussion of the existing condition of the transportation network in and around the study area is presented in the Transportation Master Plan for Route 28/Main Street prepared by McMahan Associates dated April 2016. Additional analysis is presented in the Town of Falmouth 2015 Bike Plan prepared by the Cape Cod Commission in coordination with the Falmouth Bikeway Committee.

FIGURE 4. PRIMARY ROADS IN THE STUDY AREA



NATURAL RESOURCES

The environmental setting of the Davis Straits study area is highly developed with few unaltered natural resources. The area is sewered, so development is not constrained by Title 5 septic requirements. There is very little undeveloped land within the study area, though there are small landscaped yards and buffers. Few street trees are now present, though Davis Straits was once known as the highway of elms. A brook that once connected Falmouth Inner Harbor with Morse Pond has been undergrounded through a system of culverts and pipes. According to anecdotal evidence, storm-related flooding around the intersection of Route 28 and Falmouth Heights Road may be due, in part, to the undergrounding of this stream and inadequate infrastructure to convey flood waters.

Large areas of the study area are mapped FEMA flood zones (see figure), which correspond with the historic location of the Harbor/Morse Pond stream, as well as other wetlands adjacent to the study area. The Falmouth Mall property contains a system of wetlands, once cultivated cranberry bogs, and Little Pond Brook to the east. Little Pond Brook takes form as a stream at the southern end of what is now Teaticket Park. Development in Falmouth has pressed in around the stream and its buffers, with considerable development located within the floodplain. Most of the residential properties at the western end of the Maravista neighborhood roads (Short, Swallow, Bedford, Chestnut, Priscilla, Oak, Randolph, Harrington, Montauk and Cypress Streets), as well as the Falmouth Mall buildings and parking lots, are located within the 100-year floodplain. Much greater areas are identified as part of the 500-year floodplain.

While there are few natural resources within the study area, there are numerous resources in close proximity to the site. Morse Pond is located to the west, Little Pond Brook and Teaticket Park to the north, Great Bay landing on Great Pond to the east, and Falmouth Inner Harbor to the south. The Spring Bars Road property, located on Little Pond includes 11 acres that the town has preserved and is restoring for passive recreation and access to Little Pond. Additionally, a ball field behind the Gus Canty Community Center and undeveloped woodlands around Morse Pond and associated with the Morse Pond School are located immediately west of the study area.



FIGURE 5. OPEN SPACE AND LEVEL OF PROTECTION IN SURROUNDING AREA

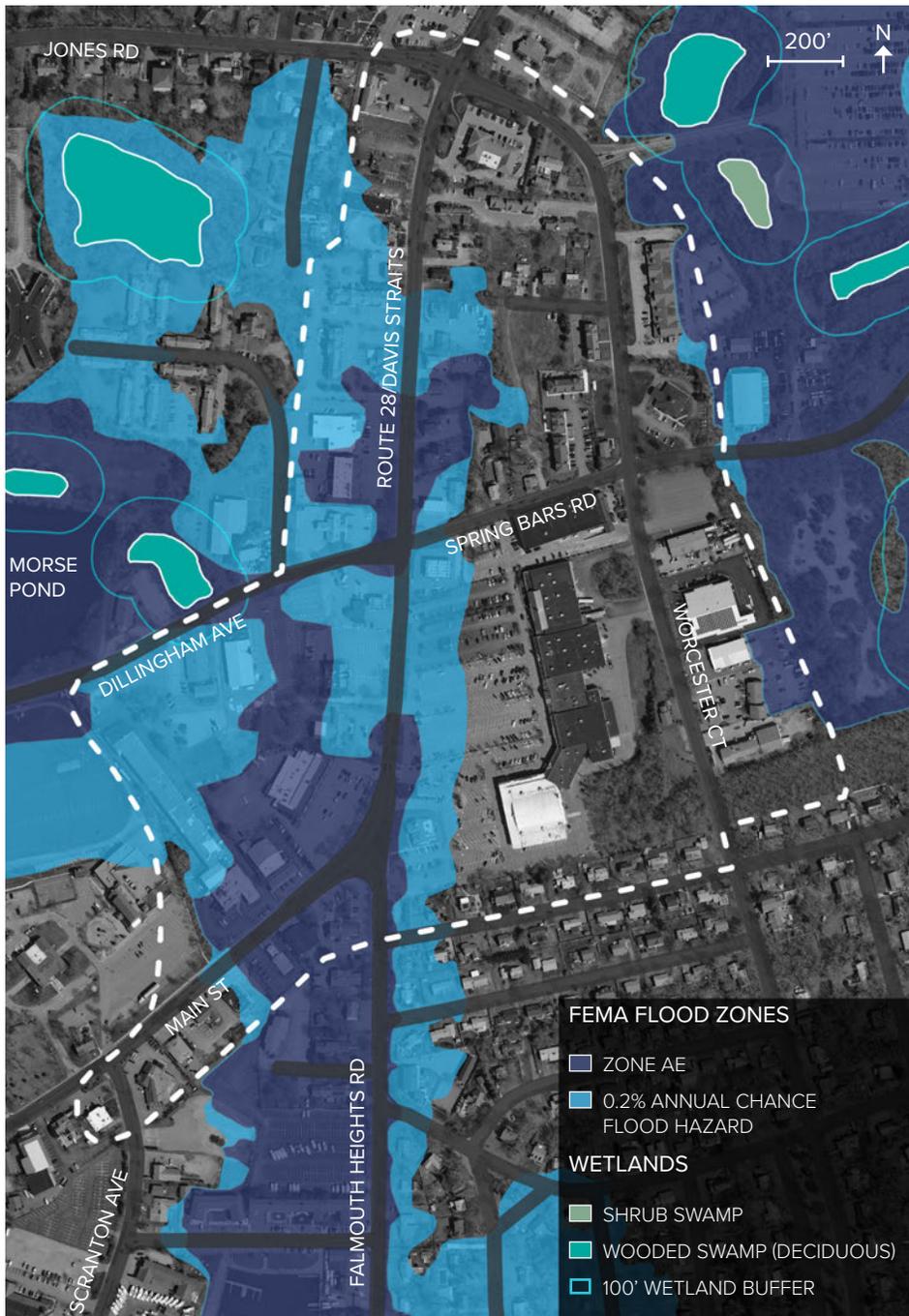


FIGURE 6. FEMA FLOOD ZONES AND WETLANDS IN AND AROUND STUDY AREA

Map Data: FEMA; Massachusetts DEP



ZONING

Zoning Districts within Study Area

There are two zoning districts within the study area: Business 2 (B2) and Business Redevelopment District (BRD). The Business Redevelopment District covers the south side of Route 28/Main Street west of Falmouth Heights Road. The rest of the study area is within the Business 2 district. The western portion of the town-owned parcel on Spring Bars Road, which is zoned Light Industrial A, abuts the study area.

Allowed Uses

The Business 2 district allows a wide range of commercial retail, office, personal and household services, and restaurants (class I, II and III). In terms of residential uses, the B2 district allows both one and two family dwellings. Special Permit uses allowed in the B2 district include:

- Commercial visitor accommodations
- Dwelling conversion to 4 dwelling units if no exterior change
- Multifamily dwellings
- Scientific research
- New business or shopping center over 7,000 sf
- Change to existing shopping center if increase of 10,000 sf or more
- Building trades activities
- Theaters and other in-door recreation uses
- Service stations
- Vehicle sales
- Parking facilities with fee
- Fast food restaurants (subject to multiple review)
- Motor vehicle repair
- Nursing home
- Wind energy, and
- Marijuana treatment centers.

The Business Redevelopment District, adopted by Town Meeting in 2004, places more emphasis on encouraging mixed use redevelopment and creating a pedestrian friendly environment. It allows combined business and residential uses under 6 units per acre so long as commercial uses occupy the first story of structures within 100 feet of the street frontage. Retail sales up to 4,000 square feet, class I and class II restaurants, and one, two and three-family dwellings are also permitted. Special permits are required in the Business Redevelopment District for:

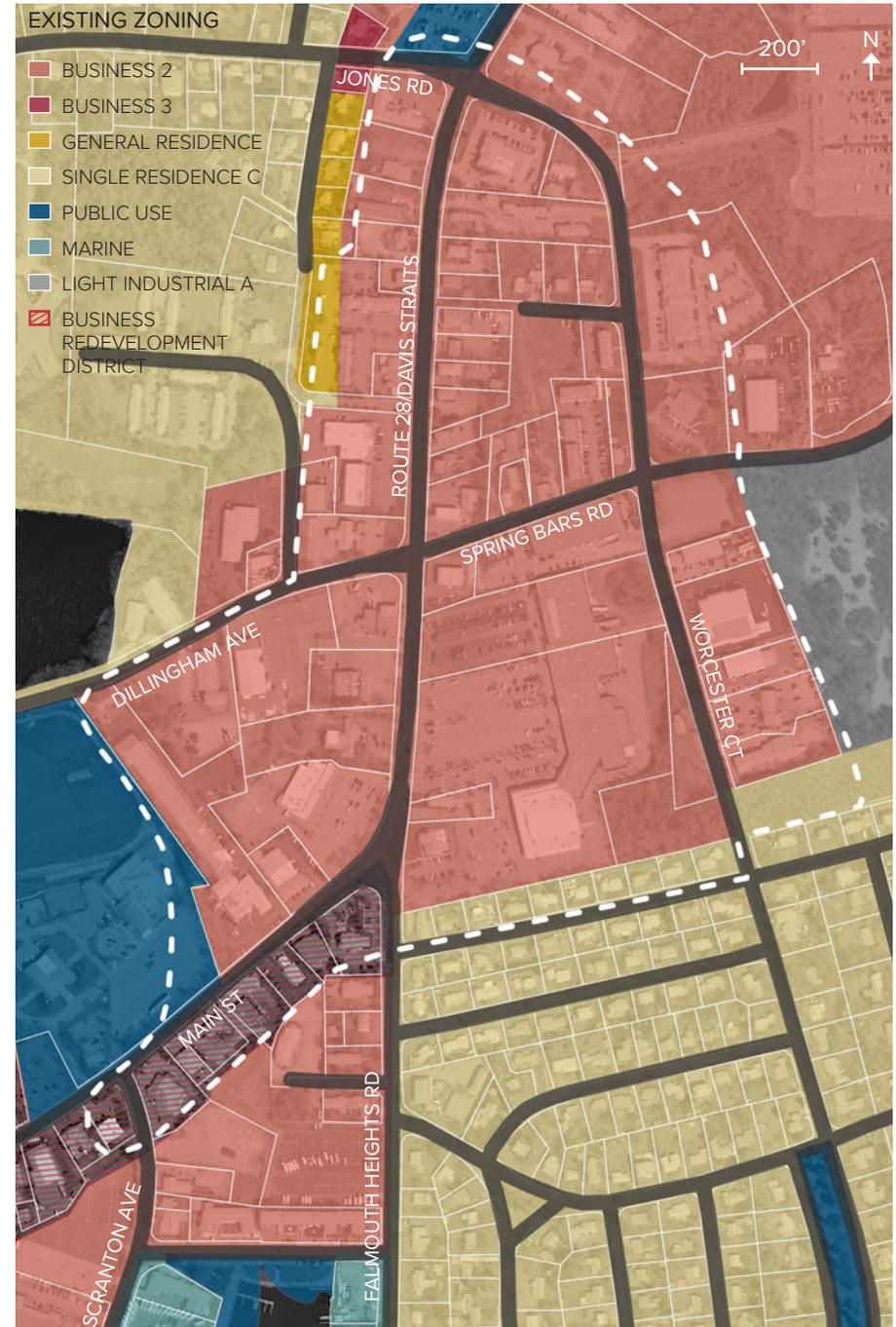


FIGURE 7. EXISTING ZONING IN AND AROUND STUDY AREA

Map Data: Cape Cod Commission Data Set

- Mixed use construction over 10,000 square feet
- Change to business use that results in a 10,000 square foot building
- Any commercial accommodations
- Multifamily uses greater than 6 units per acre and up to 8 units per acre
- Motor vehicle service stations, and
- Class III restaurants.

Dimensional Requirements

Dimensional requirements in the Business Redevelopment District are stricter than in the Business 2 district, reflecting the Business Redevelopment District goal of creating a pedestrian friendly area reflective of the earlier development pattern along Main Street with smaller, narrower lots, and smaller scale building coverage. The B2 district allows a wide range of building scales, reflected by the variety of building dimensions within the district.

Lot Size & Frontage: The B2 district has a minimum lot size of 40,000 square feet, and a minimum lot width of 200 feet, while the BRD allows smaller lots with a minimum size of 20,000 square feet and a minimum lot width of 125 feet.

Building Setbacks: Within the B2 district, the standard minimum setback is 25 feet, but on Route 28 the minimum setback is 35 feet (per Article XIV, Section 240-68). A greater setback of 50 feet is required for multifamily dwellings and commercial accommodations over 2.5 stories. Side and rear setbacks in the B2 district for commercial uses are 20 feet, though for multi-family and commercial accommodations over 2.5 stories, the side setback is equal to the building height. It is not clear how many of the existing buildings meet these setbacks.

In the BRD, setbacks are consistently smaller, with front setbacks of 20 feet, and side and rear setbacks of 10 feet. The Town has adopted specific building lines for some portions of Main Street and Davis Straits.

Lot Coverage Limits: The B2 district allows 40% building coverage and 70% lot coverage, while the BRD allows a smaller percentage of development with a maximum of 20% building coverage and 60% lot coverage.

Building Height limits: In both the B2 and BRD districts, buildings containing a primary use may be 2.5 stories not to exceed 35 feet high; accessory structures are limited to 1.5 stories and 18 feet maximum height.

Parking Location: There is no limitation on parking location in the B2 district, so large parking lots in front of buildings can define the area. The BRD allows parking only to the side and rear, and permits waivers for mixed use to allow reduced parking requirements and off-site parking within 300 feet of the property line.

The following table summarizes the dimensional, density, and parking requirements of the two zoning districts in the study area.

TABLE 1. EXISTING ZONING DIMENSIONAL REQUIREMENTS			
	B2	BRD	NOTES
Minimum Lot Size (sf)	40,000	20,000	
Minimum Lot Width	200'	125'	
Minimum Frontage	200'	100'	
Maximum Lot Coverage	70%	60%	
Maximum Building Coverage	40%	20%	
Maximum Building Height	35'	35'	
Front Setback on Route 28	35'	20'	50' for Multifamily in B2
Front Setback on other streets	25'	20'	
Side Setback	20'	10'	
Rear Setback	20'	10'	
Maximum Units per Acre	--	6	6 with Special Permit in B2 8 with Special Permit in BRD
Parking in Front	Yes	No	
Parking on the Side	Yes	Yes	
Parking in the Rear	Yes	Yes	
Off-site Parking		Yes	Allowed in BRD within 300' of property with PB or BOA approval
Shared Parking		Yes	Allowed in BRD with PB or SPGA approval

ECONOMIC DEVELOPMENT

The Existing Business Mix

Based on the Business Analyst dataset provided by ESRI, there are 152 businesses within the Davis Straits study area. Nineteen of these were not classified into industry sectors and there is no other descriptive data. Thus, for calculation purposes we are counting only the 133 classified businesses. These businesses occupy approximately 470,000 square feet of floor area; the average floor area per business in this area is 3,270 square feet with half below 2,800 square feet and half above. The largest business in terms of square footage is 22,000 square feet.

Together, these businesses employ over 840 people. The data does not indicate if this includes seasonal employees nor does it distinguish between full or part-time employees. The dataset also provides sales data imputed from national data by the type and size of the business. The sales data therefore do not reflect the seasonality of the Cape economy and have not been included in this analysis.

The most dominant single industry in the Davis Straits study area is Retail followed by Accommodations and Food Service. Over 35% of the businesses and 51% of the jobs in the study area fall into these two industries. Just under half of the commercial square footage is allocated to Retail and Accommodations and Food Service and almost two thirds of the sales documented come from these two industry sectors.

Appendix B includes a table detailing this information for all industry groups, breaking out the All Other category used in the table above. Also, a complete list of the businesses within the study area is included in Appendix B. The list includes the business name, location, and industry classification.

CHART 1. SHARES OF ESTABLISHMENTS, EMPLOYMENT, AND SPACE BY MAJOR INDUSTRY GROUPINGS IN THE DAVIS STRAITS STUDY AREA

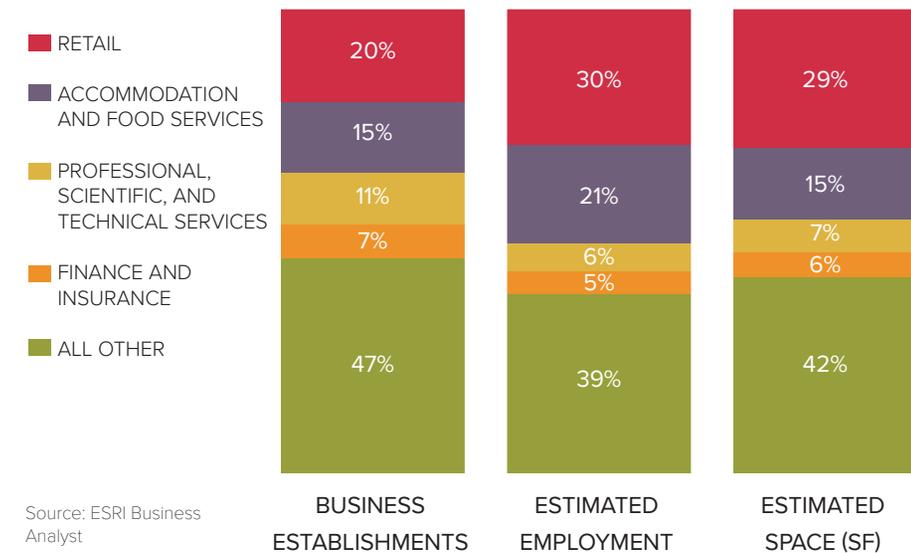


TABLE 2. BUSINESSES BY INDUSTRY SECTOR WITHIN THE DAVIS STRAITS STUDY AREA

Industry Sector (Grouped)	# of Business	% of Total	Estimated Employment	% of Total	Total Space (SF)	% of Total
Grand Total*	133	100%	841	100%	468,355	100%
Retail	27	20%	253	30%	137,711	29%
Accommodation and Food Services	20	15%	173	21%	70,909	15%
Professional, Scientific, and Technical Services	15	11%	51	6%	32,769	7%
Finance and Insurance	9	7%	38	5%	29,986	6%
All Other**	62	47%	326	39%	196,980	42%

Source: ESRI Business Analyst

* There were an additional 19 Unclassified Businesses not included in this analysis because they lacked employment, sales, and square footage data

**The All Other category includes: Administrative and Support and Waste Management and Remediation Services; Arts, Entertainment, and Recreation; Construction; Educational Services; Health Care and Social Assistance; Information; Management of Companies and Enterprises; Manufacturing; Other Services (except Public Administration); Public Administration; Real Estate and Rental and Leasing; Transportation and Warehousing and Wholesale Trade

***Sales estimates are based on national averages by industry, not actual sales. This could overestimate sales for the Cape because of our seasonal economy may limit the number of months during which businesses operate.

Existing Land Use Mix

The majority of parcels, buildings, and square footage within the Davis Straits Study Area are used for commercial activities. Government uses occupy the second largest share of acres in Davis Straits, but this doesn't translate into an equally high share of buildings or building square footage. Conversely, mixed use properties in Davis Straits occupy less land area but occupy more buildings and building square footage than government uses, as well as residential and religious uses. The 14 properties classified as purely residential occupy just 5% of the land and 4% of the total building square footage in the study area.

CHART 2. TOTAL ACRES, TOTAL GROSS BUILDING SQUARE FOOTAGE, AND NUMBER OF BUILDINGS BY SECTOR IN THE DAVIS STRAITS STUDY AREA

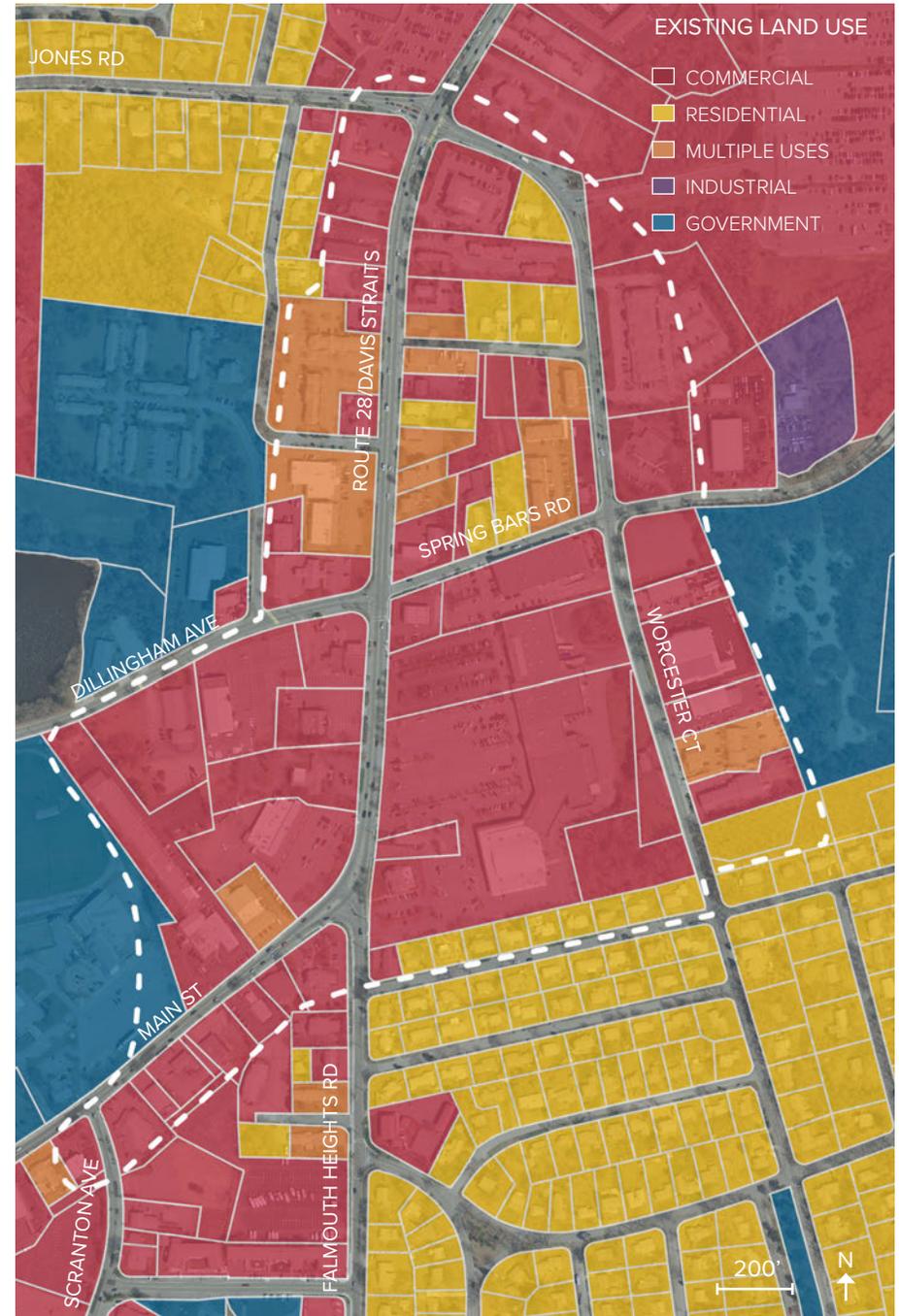
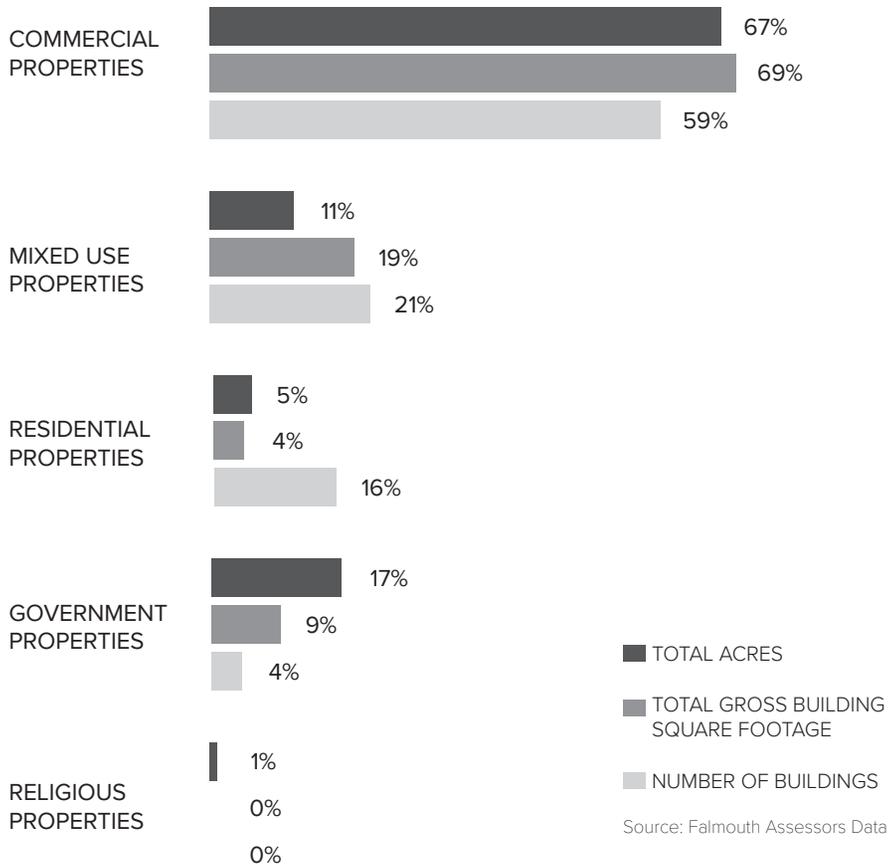


FIGURE 8. EXISTING LAND USE IN AND AROUND STUDY AREA

Map Data: MassGIS

TABLE 3. SIZE, USE, AND VALUES OF PROPERTIES IN THE DAVIS STRAITS STUDY AREA

	Total	Commercial Properties	Mixed Use Properties	Residential Properties	Government Properties	Religious Properties
Total Acres	79.6	53.4	8.5	3.7	13.4	0.6
Total Gross Bldg SF	793,220	544,848	147,144	31,074	70,154	0
Number of Buildings	90	53	19	14	4	0
Number of Bedrooms	240	156	47	37	0	0
Total Assessed Value	94,479,100	60,597,500	13,377,400	2,983,900	17,123,000	397,300
Average Assessed Value	1,111,519	1,063,114	1,029,031	229,531	17,123,000	397,300
Properties Sold	85	57	13	13	1	1
Average Sale Price	384,741	432,151	512,862	107,938	0	0
Total Sales	32,703,025	24,632,624	6,667,201	1,403,200	0	0

Source: Falmouth Assessors Data

TABLE 4. ASSESSED VALUE OF PROPERTIES IN THE DAVIS STRAITS STUDY AREA

	All Properties	Commercial Properties	Mixed Use Properties	Residential Properties
Total AV/Acre	1,186,923	1,134,785	1,573,812	806,459
Property Sale Value/Acre*	410,842	461,285	784,377	379,243
Total AV/Gross Bldg SF	119	111	91	96
Total AV/Building	1,049,768	1,143,349	704,074	213,136
Gross Bldg Area/Acre	9,965	10,203	17,311	8,398
Gross Bldg Area/Bldg	8,814	10,280	7,744	2,220
Buildings/Acre	1.1	1.0	2.2	3.8

Source: Falmouth Assessors Data

* These values are based only on the properties with sales data

In terms of total taxable assessed value, commercial uses constitute 64% of the total for the study area; when the assessed value of government and religious properties is not included this share increases to 79%. Mixed use properties contribute the next largest share (14-17%) while residential properties contribute less than five percent of the area's total assessed value. The share of property sales and sale proceeds are similarly distributed across commercial, mixed use, and residential properties.

CHART 3. RELATIVE VALUE OF DIFFERENT LAND USES IN THE DAVIS STRAITS STUDY AREA



Source: Falmouth Assessors Data

While there are not many mixed use properties in Davis Straits, the data indicates that they have the largest per acre value both in terms of assessed value and property sale value. The difference between mixed use and pure commercial or pure residential is significant for both these measures.

Another difference between the types of properties in Davis Straits is the average building size. Commercial buildings are by far the largest on average and residential, not surprisingly, the smallest. Interestingly, while mixed use buildings are smaller on average, they accommodate the most building square footage per acre as compared to commercial uses, possibly because they provide less parking.

CHART 4. AVERAGE BUILDING SIZE (SF) BY TYPE IN THE DAVIS STRAITS STUDY AREA



Source: Falmouth Assessors Data



**ISSUES AND
OPPORTUNITIES**

BUILT ENVIRONMENT

While the Davis Straits study area is highly developed with commercial uses today, relics of its early history remain. Older residential-scaled historic structures found along Route 28 provide a remnant of the former residential uses found within the Davis Straits area. These structures, which now contain a mix of business and residential uses, are modest in scale and line a section of the roadway with small front yards and parking areas to the rear. Efforts to retain the scale and protect the viability of these historic structures should be provided through zoning or other incentives.



SOME OLDER RESIDENTIAL PROPERTIES REMAIN IN THE AREA

The Gus Canty Community Center, formerly the USO building, is a community gathering place for residents. Fuller Field, located directly behind the Gus Canty Community Center, is also heavily used during the summer months and is home to the Falmouth Commodores. The Falmouth public safety complex is also located on this town-owned site. The site is centrally located and has ample parking. There is the potential for additional development to the front of the site with improvements to existing site circulation and parking that could generate even more year-round activity in the Davis Straits area.

Much of the development along Route 28 in Davis Straits dates from the mid-20th century, when it took the place of earlier agricultural fields. Many of these commercial developments have large lots and large building footprints, defined by large parking areas along the road frontage. Especially with these large lots, there is potential for the town to direct future development or redevelopment efforts by creating effective regulations and design guidance.



EXAMPLE OF AUTO-ORIENTED DEVELOPMENT ON DAVIS STRAITS

Despite the challenges presented by Route 28, the Davis Straits corridor also presents a great deal of opportunity from a transportation perspective. There appears to be sufficient business activity that, if sufficient infrastructure existed, the area could become an active pedestrian corridor. There are a number of ongoing and planned transportation improvements in the area, including the proposed roundabout at the intersection of Route 28, and improvements to the intersection of Jones Road and Worcester Court that will improve safety and provide a gateway to the Davis Straits area.

Davis Straits is also currently connected to sewer, and therefore development is not constrained by Title 5 septic system requirements. However, Davis Straits is located within the watershed to Little Pond, a nitrogen-overloaded embayment, so consideration for nitrogen management and water quality in Little Pond should be a major goal.

NATURAL SYSTEMS

Some of the key natural resource assets found within Davis Straits are hidden from view or lack connections to other public amenities. For example, the Falmouth Inner Harbor is actively used for local recreational fishing, boating, and water transportation, but it is physically and visually disconnected from the downtown and the Davis Straits area. Improved pedestrian and bicycle amenities to connect the harbor to Davis Straits and Morse Pond could encourage visitors and residents to enjoy these amenities and foster redevelopment of adjacent commercial uses.

Teaticket Park provides visual relief and a public amenity within the business district, and the Spring Bars Road property at the southern end of the study area holds great potential as a natural and recreational asset. The town is working on a management plan for the Spring Bars Road parcel to improve it for recreational access, including a kayak launch, walking trails, and educational access to the Atlantic White Cedar swamp on the property. The 300 Committee has initiated a plan to restore wetlands and meadow habitat on the Teaticket Park site, and provide walking trails. The town also owns a right of way (ROW) along the eastern side of Little Pond Brook, extending between Maravista Ave and Randolph Street.

Based on conversations with stakeholders, including The 300 Committee, Conservation Commission staff, and neighborhood residents, there appears to be support for establishing a linkage between Teaticket park and Spring Bars Road. Most agreed that this linkage may be appropriate for walking trails, and likely not for biking or a multi-use path. Stakeholders supported re-establishing the wetlands and buffers along the brook, and supported increasing the protected open space to establish and formalize a greenway for the purposes of floodplain protection and habitat restoration.

The Town's desire to link these two natural areas with a greenway corridor presents an opportunity to restore portions of the study area to a more natural state, can help improve water quality, and can also be a catalyst for redevelopment and general improvement of the area.

The Falmouth Mall, just outside of the study area, contains large wetlands that were formerly cranberry bogs that were partially filled during development of the Mall. Given changes in climate that are bringing stronger intensity storms, as well as the prospect of sea level rise, the town should consider actions that restore these traditional floodplains to a flood storage function, and discourage any new development in these areas.



REGULATORY FRAMEWORK

Zoning Opportunities

The Davis Straits study area is a large area with varied characters, yet it is mostly regulated by a single zoning district. Changes in zoning district boundaries, allowed uses, building scale, building setbacks, and parking location are all ways in which development in Davis Straits could be guided to support more variety and to highlight different characteristics in different locations.

The extent of the B2 zoning designation in the study area should be reconsidered. There is a wide range of uses and scales of development allowed throughout the B2 Zoning District. Other business zoning district designations in Falmouth could be applied to portions of the study area, or new zoning districts or overlays could be created to differentiate and support the varying characters within the study area. Identifying the desired uses to be allowed or prohibited, and the appropriate building scale for each neighborhood area will be an important part of creating appropriate district regulations. Some of these uses could be limited to certain portions of the district in an effort to better guide community character and create smaller neighborhoods with unique characters. One idea is to limit auto service uses from key intersections. In addition, new special permit criteria could be developed to support specific characteristics that are desired.

Changing front setback requirements to bring buildings closer to the street can have a dramatic impact on the character of the area. This will be an important component of guiding parts of the study area to change from commercial strip style development to a neighborhood or pedestrian friendly style development.

Prohibiting parking in the front of the lot, along with changes in building setbacks, is another key element in changing the character of the area from strip development to pedestrian friendly development. Numerous business districts in town, as well as the Light Industrial A district, already require parking to the side or rear of a development.

Form-Based Code

Standard zoning, like that found in the Davis Straits study area, divides the community into sectors, with separate zones for apartments, single-family

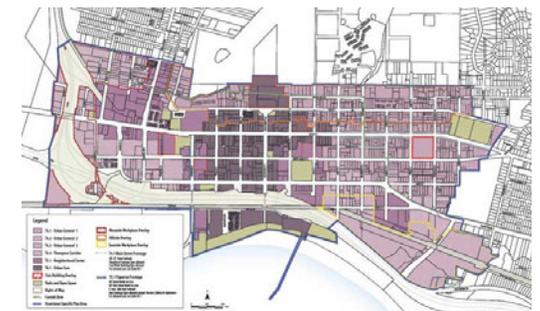
homes, shopping, offices, and industry. To move among these zones, everyone needs to drive, resulting in the sprawling development pattern we see today.

The town has expressed an interest through its Local Comprehensive Plan in exploring an alternative regulatory approach called form-based code. Form-based codes offer a new way of thinking about development regulation to support more vibrant mixed use centers supporting businesses large and small, with neighborhoods and streets that are safe and attractive for walking and bicycling, and that protect the community's historic values and the environment.

Form-based codes are a set of design regulations that place emphasis on the form of the built environment, streets, and open space, rather than use as a primary driver of development. Form-based codes are increasingly being used to retrofit older downtowns and commercial areas to encourage more mixed use and pedestrian friendly neighborhoods.

Creating a form-based code begins by asking a community to envision its future through an illustrative plan that shows building types, the relationship of buildings to public spaces, the types and location of streets, the placement of parking, the size of blocks, and more. Once a community adopts an illustrative plan, a form-based code is drafted to implement the plan. The code contains standards that are required of all new development, not just guidelines.

Form-based code could be one possible approach to meet the Town's goals for the area. The concept plans and illustrations prepared by staff provide an initial step towards adoption of an illustrative plan by the Town should they decide to pursue this approach. Additional effort would be needed to ensure broad support and input by the community prior to adopting a vision for the area.



Source: Massachusetts Smart Growth/Smart Energy Toolkit

ECONOMIC DEVELOPMENT

Just as the character of an area is heavily determined by zoning and other public policies, so too is the economic activity within an area. Zoning directly impacts the supply of land by delineating how much land on a given property may actually be used to generate revenue and how much must be carried as overhead. Zoning also constrains demand by limiting the uses possible in an area. Public policies and investments relative to infrastructure have a significant impact on the value of properties, the cost to develop these properties, and the potential for a sufficient return on investment. This in turn impacts what types of development will make financial sense in specific locations and which do not.

The following analysis considers the economic value of Davis Straits in this context in order to understand the impact of existing policies relative to the goals stakeholders have expressed for the Davis Straits study area, and to recommend possible changes that have a greater chance of supporting the realization of these goals.

Expressed Economic Development Goals for Davis Straits

Through a variety of meetings with policy makers, residents, and business interests, a number of goals for the Davis Straits study area have emerged. These stakeholders would like to see in-fill and redevelopment in the area that will:

- Add living wage employment options
- Provide retail & services needed by year-round residents
- Provide residential options that are affordable for year-round residents of all incomes, ages and backgrounds
- Limit further demand on existing road and sewer infrastructure, and
- Improve the character of the area to be more reminiscent of a pedestrian oriented village center.

The table below outlines the relationship between these goals and zoning and related public policies. It provides some explanation of how policies may impact the type of business activity and land uses found in Davis Straits now and in the future.



TABLE 5. ECONOMIC DEVELOPMENT GOALS

Economic Development Goals	Local policies that impact these goals	Why can these policies be limiting?
Encourage in-fill & redevelopment	Zoning coverage limitations, parking and dimensional requirements and density limitations; Complexity and cost of permitting process	High up-front costs and limits on the percent of a property that can be used to generate revenue, especially given high land costs, can make redevelopment or new in-fill development financially infeasible
Add living wage employment options	Zoning use limitations and form requirements; lack of public investment in necessary infrastructure and community character; permitting strip malls which are only suitable for retail	As markets change, communities with building stock that can be used for a wide variety of uses and who have invested wisely in infrastructure and community character will be in a better position to evolve successfully with changes in demand
Offer a retail and service mix needed by year-round residents	Licensing, zoning, formula business by-laws	Many more businesses exist in seasonal markets than the local resident population can support. Limiting off-season supply focuses demand in a way that allows local business operators to stay open year-round
Provide year-round rental housing options that are broadly affordable	Residential density requirements in zoning, as well as dimensional and coverage limitations and parking requirements; lack of sufficient wastewater infrastructure	If developers are limited in the number of apartments they may create and can only earn revenue off a limited portion of the land they must carry, the cost of those units will need to be higher to cover their costs; this may be further exacerbated by a lack of public infrastructure
Reduce or limit negative impacts to infrastructure (in particular roads and sewer)	Zoning and other policies that result in dispersed, low density, auto dependent development patterns that cannot be easily served with public infrastructure such as sewer or transit	Low density actually places a greater burden on the road infrastructure and makes the provision of transit and wastewater more expensive and less effective
Improve the area's appeal and character	Policies that encourage public investments in the public realm – streetscapes, parks, open spaces, and public buildings – can significantly increase the economic success of an area. Similarly, a lack of such investment can contribute to the decline of an area	The location of public facilities can act like an anchor store to a village or town center. Investments in the streetscape and open spaces can significantly increase the value of the proximate properties as they create comfortable human spaces that draw people to live, work, shop, and interact in the area

Key Findings

To understand Davis Straits from an economic perspective, the area was compared to a roughly equally sized area in the center of the village and to the entire town of Falmouth. Businesses were compared both by type and location to understand differences in business mix, employment, and commercial space used. Properties were compared by land use (commercial, residential, and mixed use) and by location. The data was “normalized” to allow a comparison of the relative value of different business types and uses across the different locations. The comparison metrics for business comparisons were businesses per acre, average building square footage by business type, employees per business and per square foot.

In comparing the relative value of land uses by type of property or location, the properties were compared by total assessed value per acre, per gross square foot, and per building, as well as by gross square foot per acre and per building, buildings per acre, and, for residential, bedrooms and kitchens (a proxy for the number of units) were used to compare total assessed value, size, and units per acre. The most recent sale price for those properties with that data was similarly evaluated on a per acre, per building, per square foot, per bedroom, and per unit basis.

This study relied on three datasets to complete the economic development analysis:

1. Business Analyst, ESRI
2. Quarterly Employment and Wages (QEW or ES202), US Bureau of Labor Statistics
3. Property Tax Assessors Data, Town of Falmouth

There are three significant findings that should be understood and used to inform any changes to zoning or other policies pertaining to Davis Straits.

1. Davis Straits Business Mix is Dominated by Retail and Accommodations & Food Service. Retail businesses occupy the majority of space within the Davis Straits study area and constitute the largest group of businesses with the most employees, followed by businesses classified as Accommodations and Food Service. The Village Center is similarly dominated by these same types of businesses. Wages in these two sectors are the lowest across sectors in Falmouth and Cape-wide. Typically, many of the jobs in these two sectors are also part-time and seasonal.
2. Businesses in Davis Straits are Large in Size with Fewer Employees per Square Foot. Retail businesses in the Davis Straits study area are physically larger on average than other types of private businesses in this location. They are also significantly larger than retail businesses in the Village Center or elsewhere in Falmouth. Retail businesses occupy the largest amount of commercial space of any business sector in Falmouth; they are not, however, the largest sector in terms of employment. The leading employment sectors in Falmouth are (1) Professional, Scientific, & Technical Services and (2) Health Care and Social Assistance.
3. Greater Density pays off in Higher Assessed Values and Sale Prices per Acre. The Village Center has the highest assessed value per acre for commercial, mixed use, and residential properties in Falmouth. Similarly, sale price per acre is higher in the Village Center with one exception - the two multi-family housing properties that sold within the Davis Straits Study area had higher sales value per acre than the seven properties sold downtown.

Economic Development Issues & Opportunities Summary

Observations of the study area, discussions with stakeholders, and the findings outlined above can be summarized into a short list of economic development issues confronting Davis Straits and the opportunities that exist to improve the economic development performance of the area in the future.

Economic Development Issues

- Davis Straits and the Village Center both depend heavily on retail and food service businesses. This sets up an uneven competition between the small, locally owned businesses downtown and the well-known, well-funded national retailers in Davis Straits.
- The business mix in Davis Straits provides job opportunities that are primarily low wage, that is, offering wages below the local cost of living.
- Davis Straits is dominated by non-locally owned businesses that spend more of their earnings off-cape than do locally owned businesses.
- Trends in retail and housing demand may make the auto-oriented strip development characteristic of Davis Straits obsolete and possibly blighted with empty properties
- Low density development in Davis Straits results in forgone tax revenue and sales value on a per acre basis.

Economic Development Opportunities

- Allowing more housing development will increase the local customer base and earnings potential for businesses in the area.
- If locally-owned businesses are encouraged a greater amount of business earnings will be spent locally.
- In most cases, the nature and scale of the businesses in Davis Straits do not require low development density or even auto-oriented infrastructure to function, thus changes to this area need not crowd existing businesses.
- In-fill could provide a greater variety of commercial and residential options and price points while improving the character of the area.
- Increasing the concentration of residential and business activity in Davis Straits will use the land more efficiently and result in both higher revenue to the town and sales value to the developers.



OUTREACH

VISIONING MEETING

Commission staff attended a joint meeting of the Falmouth Board of Selectmen and Planning Board in March 2016 to discuss the vision for the study area. Participants indicated a desire to create a visually and functionally pleasant part of town that continues to provide goods and services needed by the residents of Falmouth, but in a setting that is more pedestrian and bike friendly and less auto-oriented.

There was interest in creating a transition between the Business Redevelopment District to the west of the study area and a redefined Davis Straits district from Falmouth Heights Road to Jones Road/Worcester Court. Participants valued the scale, form, and placement of the buildings along the eastern side of Davis Straits between Jones Road and Spring Bars Road. Participants also wanted to preserve and expand the small front yard setbacks to allow for landscaped green space or café seating, and to have parking located to the side or rear. There was substantial support for mixed use, including residential and retail or office, and less concern about specifying the kind of uses within the district.

Participants wanted to improve safe pedestrian and bicycle transit, reintroduce street trees, reduce curb cuts, and improve access to and provision of open space along Route 28. Participants wanted to continue support of the light industrial uses located along Worcester Court, but improve pedestrian amenities and transitions to neighboring residential areas.

STAKEHOLDER MEETINGS

In May and June 2016 Commission staff met with stakeholders in the town to discuss their interests in and vision for the Davis Straits area. Commission staff met with members of The 300 Committee, Town Conservation staff, the EDIC, the Falmouth Chamber of Commerce, representatives of the Save Little Pond Association, Falmouth Heights/Maravista Improvement Association, and the Teaticket Civic Association.

Stakeholders were interested in making the study area more appealing, more walkable, and improving its character. Stakeholders felt that the area suffers from too many drive-thrus, gas stations, and a lack of landscaping. The large parking lots adjacent to the street, utility lines, and sign clutter were also identified as problematic. They also felt that the design of buildings could be improved with better materials, less glass, and encouraging a Cape Cod building character. They indicated comfort with two story buildings, similar in scale to those found at Mashpee Commons, and with buildings close to the road edge. Stakeholders wanted to see improved access to and within the study area, both for pedestrians and cyclists as well as vehicles. Stakeholders supported more mixed use development including residential, but were less supportive of more multi-family residential developments.

Stakeholders also expressed the hope that this area might provide employment and housing opportunities for young families and middle income households. Similarly, they hoped the retail mix would provide goods and services needed by year-round middle income residents. With the goal of diversifying the local economy and providing higher-wage jobs, the stakeholders suggested reviewing zoning requirements and permitting processes to identify changes that could entice high-wage high-skill employers to locate in the Davis Straits Area.

VISUAL PREFERENCES SURVEY

As part of an October 2016 Planning Board workshop, Commission staff conducted a survey of visual preferences to assist with understanding participants' preferred scale and form of future development, as well as the preferred scale and types of open space to be encouraged within the Davis Straits area. Commission staff divided the study area into four sub-areas based on differences in the existing scale and massing of development.

These areas consisted of the following:

1. Route 28 – Worcester Court to Dillingham Avenue
2. Route 28 – Dillingham Avenue to Falmouth Heights Road
3. Transition Zone on Route 28 from Falmouth Heights Road to the Business Redevelopment District
4. Worcester Court

For each sub-area, participants were shown a series of images and asked to assign their preferences for each of the following:

- Building massing
- Front yard setbacks
- Front yard treatment
- Parking location
- Major intersections – scale and use
- Mixed use or single use
- Scale of open space

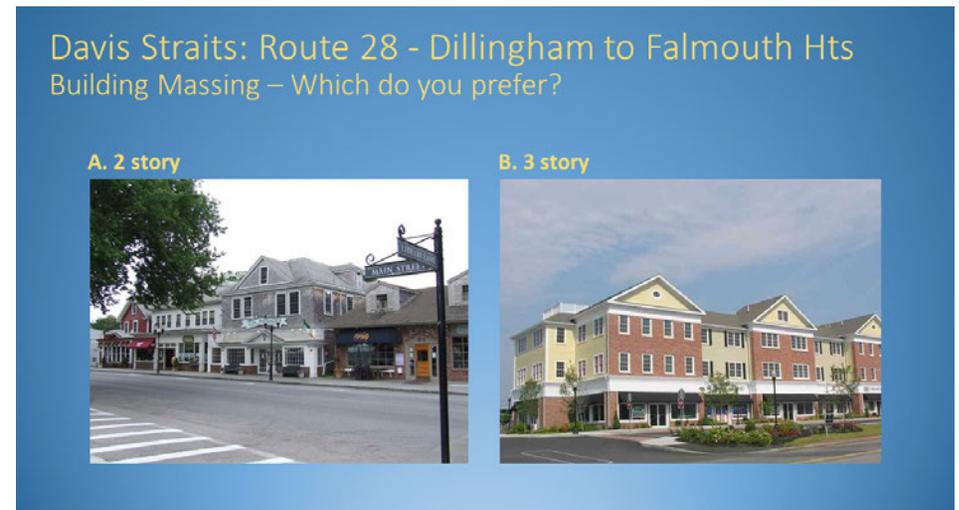
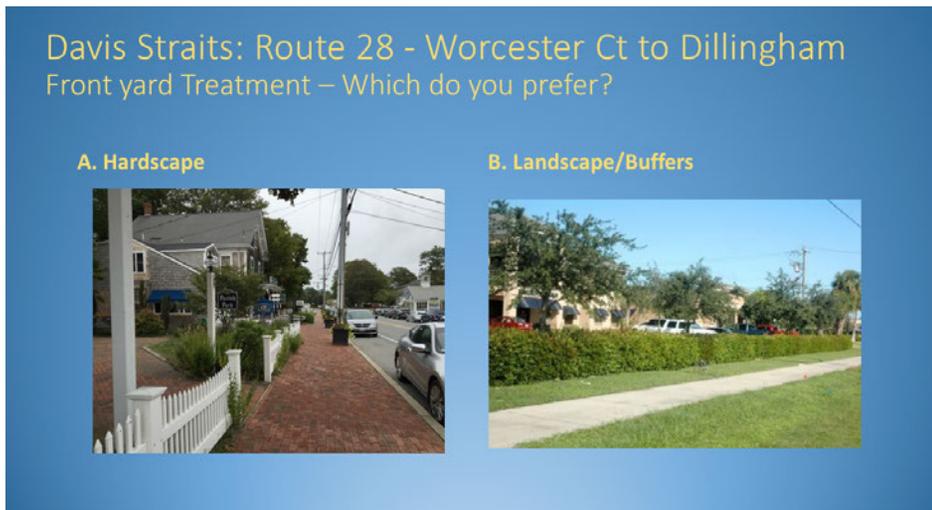
Survey Results

Although there were a limited number of participants at the workshop, the visual preference survey yielded some useful insights that staff carried forward when developing the conceptual redevelopment plan. For instance, throughout the study area, participants unanimously preferred parking to the side or rear of future development as opposed to parking in front with a landscape buffer. Participants also did not want to see major intersections defined by larger commercial development. Instead, they would like to see larger mixed use buildings or perhaps something else not shown. Participants also strongly preferred mixed use over single use office or retail buildings throughout the study area. Participants also felt that each of the open space types shown could be beneficial throughout the study area, depending on the specific location.



FIGURE 9. AREAS FOR VISUAL PREFERENCES SURVEY

FIGURE 10. EXAMPLES FROM VISUAL PREFERENCES SURVEY



In addition to these general observations, the results of the visual preference survey are generally described below, broken down by each sub-area:

1. Route 28 – Worcester Court to Dillingham Ave.

There was a slight number of participants who preferred a two-story larger footprint over a smaller two-story footprint; small front yard setback; hardscape front yard treatment; parking located to the side or rear; larger mixed use buildings or something else at intersections; a preference for mixed use; and that each of the open space types could be appropriate in this area.

2. Route 28 – Dillingham Ave. to Falmouth Heights

In this area, two-story development was slightly preferred over three-story buildings; small front yard setbacks; parking to the side/rear; mixed use or something else at intersections; mixed use residential with retail strongly preferred; and a mix of open space types.

3. Transition Zone on Route 28 from Falmouth Heights to the Business Redevelopment District

Participants unanimously favored two-story over three-story development; small front yard setbacks; were evenly divided on hardscape or landscape front yard treatment; preferred parking to the side or rear; were unclear about the scale at major intersections, indicated by the slight preference for “something else” not

shown; indicated a strong preference for mixed use; with a strong preference for pocket parks over other open space types.

4. Worcester Court

Participants were evenly split on the preferred scale of development between smaller or larger building massing; unanimously preferred small yard setbacks over none; preferred a landscape treatment for front yards over a hardscape treatment; slightly preferred off-street over on-street parking; indicated a strong preference for neither large-scale commercial nor larger mixed use buildings at intersections (preferring something else not shown); slightly preferred mixed use over multi-family; and unanimous that pocket parks were most appropriate in this area.

Survey Feedback

Feedback from workshop participants indicated that additional photos should be included in the survey that better illustrate Cape Cod development forms, and provided other useful input for potential improvements to the survey. Staff suggests that the survey could be retaken with a larger stakeholder group or made available on-line to verify these preliminary results.



**A VISION
FOR THE
FUTURE**

Based on the substantial public outreach conducted as part of this study, including input from the Falmouth Board of Selectmen, Planning Board, and numerous local committees and stakeholders, Commission staff has developed a preliminary vision statement for the Davis Straits area as follows.

OVERALL VISION

The Davis Straits area will be a comfortable place for residents to live, work, and shop, characterized by:

- Pedestrian scale development
- A variety of commercial and housing types and sizes
- An affordable mix of commercial space, residential options, and goods and services
- Safe roadways with pedestrian and bike amenities, reduced visual clutter and curb cuts, and increased tree cover and landscaping.

Redevelopment of the area also envisions a greenway network that provides visual and physical access to the water, flood storage capacity, increased recreation opportunities, and improved pedestrian and bicycle access to and from nearby densely developed residential areas.

CONCEPTUAL REDEVELOPMENT PLAN

As required by the scope of work, Commission staff has prepared a conceptual redevelopment plan for the study area. The conceptual redevelopment plan divides the study area into five distinct character areas and a proposed hierarchy of streets and ways to help define these character areas.

These character areas reflect the town's overall desire to differentiate the Davis Straits area from other commercial areas and to provide more mixed use development. Character areas are based on the proposed scale and form of potential redevelopment and are intended to provide an initial framework for further discussion by the Town about future zoning changes to bring about the desired character of development.

For each of the character areas, staff has provided a description and graphics illustrating the relationship of buildings to the street front, building scale, and the location of parking. Some of the proposed character areas create districts that resemble residential neighborhoods in their modest scale, moderate setback and front landscaping, while others could accommodate larger buildings and shared parking. In addition, the commercially oriented character areas define the desired amount of building enclosure (the amount of the road frontage that must be defined by a building) and the required amount of building transparency (the amount of window space that is required for first floor commercial uses) in order to create more pedestrian-oriented places.

FIGURE 11. CONCEPTUAL REDEVELOPMENT PLAN

CHARACTER AREAS

- DAVIS STRAITS COMMERCIAL CLUSTER
- VILLAGE MIXED USE
- RESIDENTIAL MIXED USE
- NEIGHBORHOOD COMMERCIAL CLUSTER
- GREENWAY

HIERARCHY OF STREETS

- PRIMARY STREET
- SECONDARY STREET
- ⋯ NEIGHBORHOOD/CONNECTOR STREET
- ✱ PROMINENT INTERSECTION

■ 500 YEAR FLOOD PLAIN

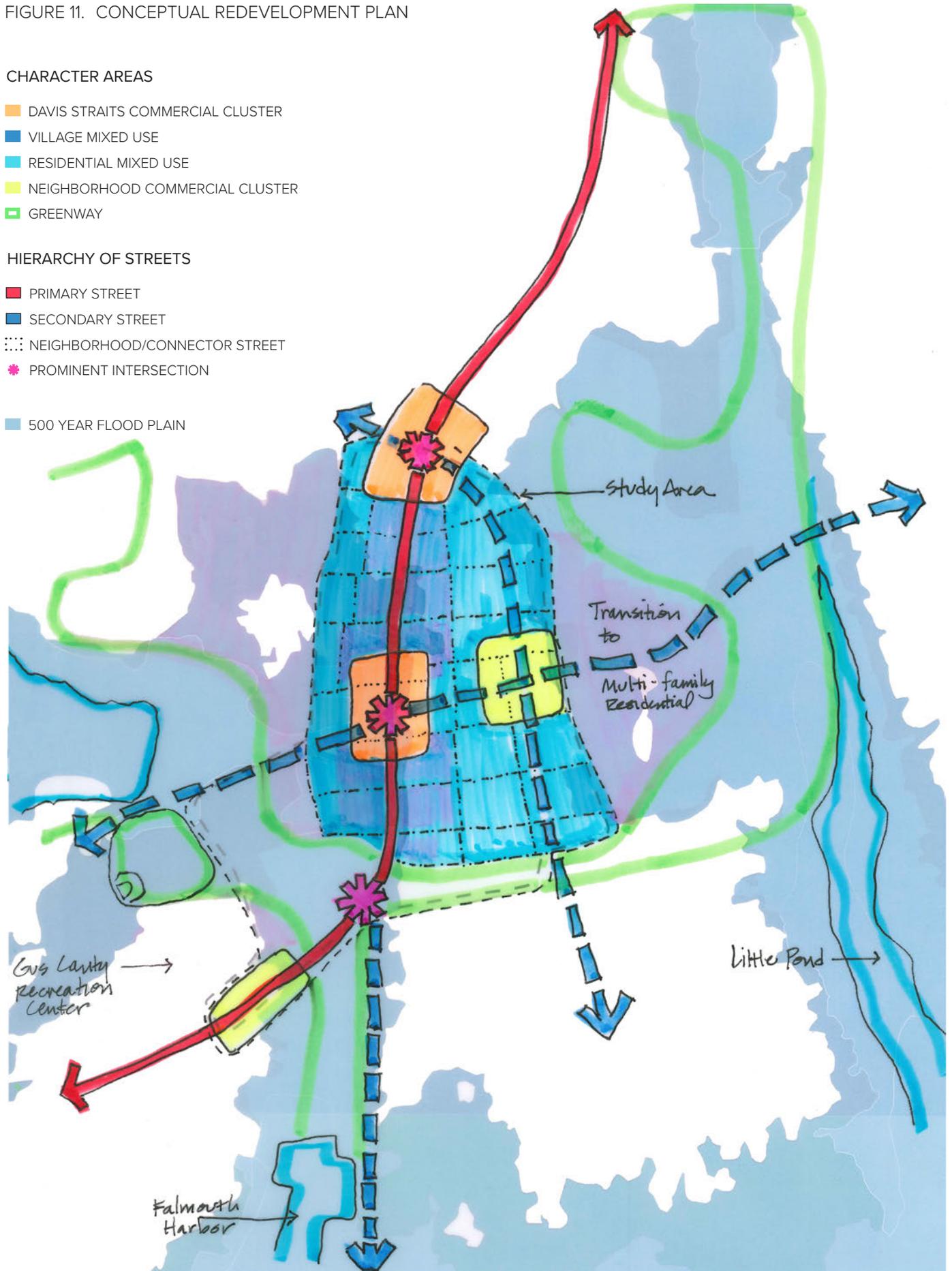


TABLE 6. POSSIBLE DIMENSIONAL REQUIREMENTS FOR CHARACTER AREAS

Character Area	Base Residential Density	Front Setbacks	Side and Rear Setbacks	Maximum Building Height	Maximum Building Coverage	Minimum Lot Size (sf)	Maximum Building Footprint (sf)	Building Enclosure of Frontage on Principal Street	Building Transparency for Street Facing Facades	Parking Setback	Parking Placement
Davis Straits Commercial Cluster	--	10' to 20'	10'	2 stories or 30'	40%	20,000	10,000	70% minimum at setback	40% of ground floor transparent	50'	Rear
Village Mixed Use	5 dwelling units per acre	5' to 15'	5'	2 stories or 25'	20%	5,000	2,000	--	--	20'	Side or rear
Residential Mixed Use	12 dwelling units per acre	15'	10'	2.5 stories or 30'	30%	10,000	15,000	--	--	20'	Rear and street
Neighborhood Commercial Cluster	5 dwelling units per acre	10'	10'	2 stories or 25'	30%	10,000	5,000	70% minimum at setback	40% of ground floor transparent	20'	Side or rear

CHARACTER AREA DESCRIPTIONS

Davis Straits Commercial Cluster

Areas located at key road intersections on Route 28 with a mix of two-story retail and office development located close to the street edge or with small front yard setbacks that may be landscaped or landscaped areas. Parking located behind buildings. The building envelope encompasses the front half of the lot on both street fronts; parking envelope encompasses the back half of the lot on both street fronts. Buildings must have transparency at the street front and must enclose much of the corner frontage.

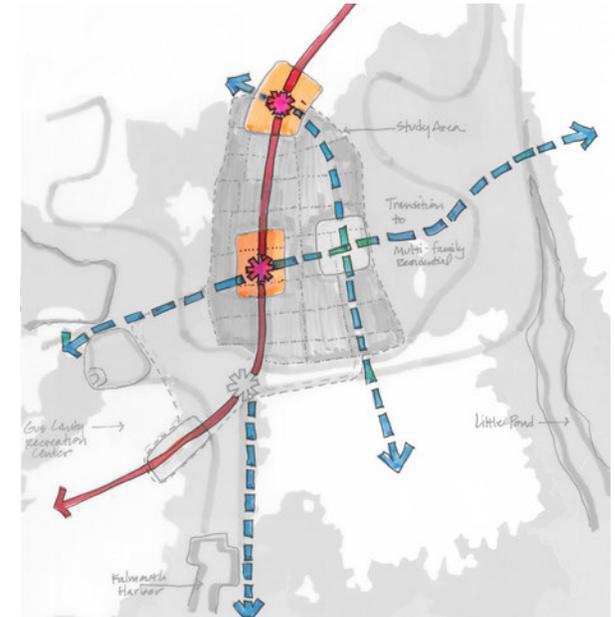
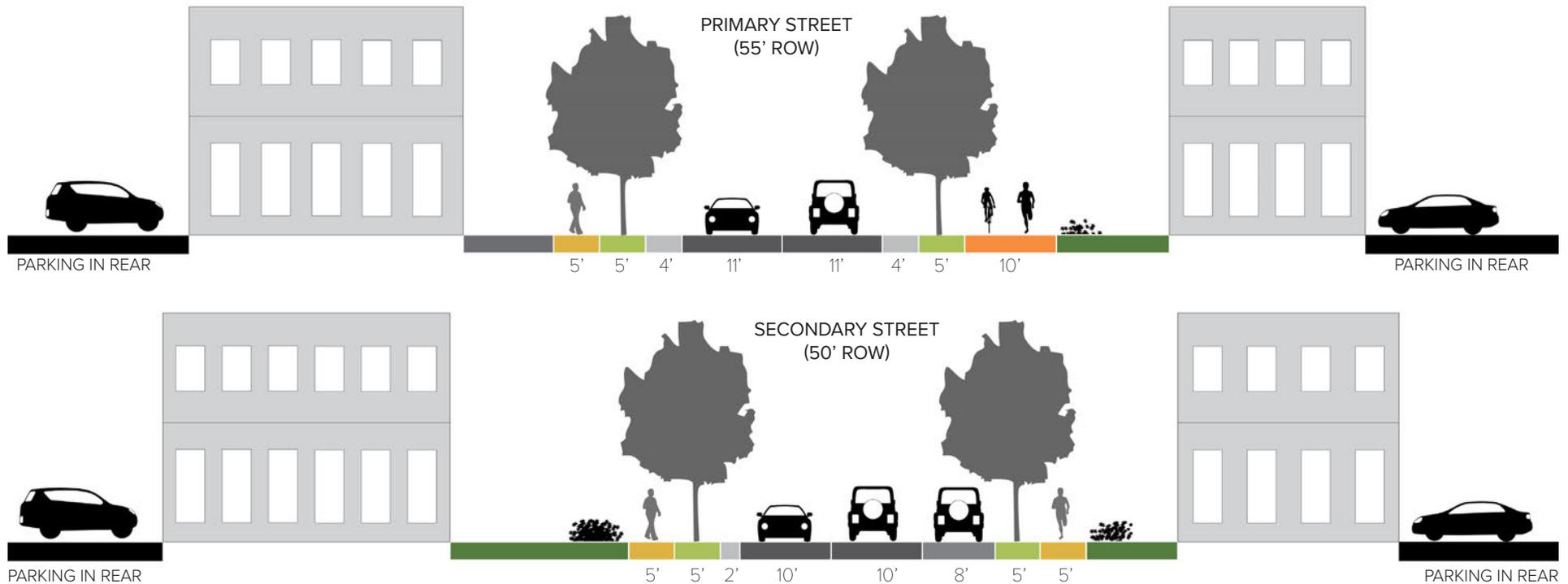


FIGURE 12. EXAMPLE STREET SECTIONS FOR DAVIS STRAITS COMMERCIAL CLUSTER AREAS

STREET SECTIONS

- SIDEWALK
- MULTI-USE PATH
- ON-STREET PARKING
- BUFFER
- SHOULDER
- TRAVEL LANES



Base Residential Density	--
Front Setbacks	10' to 20'
Side and Rear Setbacks	10'
Maximum Building Height	2 stories or 30'
Maximum Building Coverage	40%
Minimum Lot Size (sf)	20,000
Maximum Building Footprint (sf)	10,000
Building Enclosure of Frontage on Principal Street	70% minimum at setback
Building Transparency for Street Facing Facades	40% of ground floor transparent
Parking Setback	50'
Parking Placement	Rear

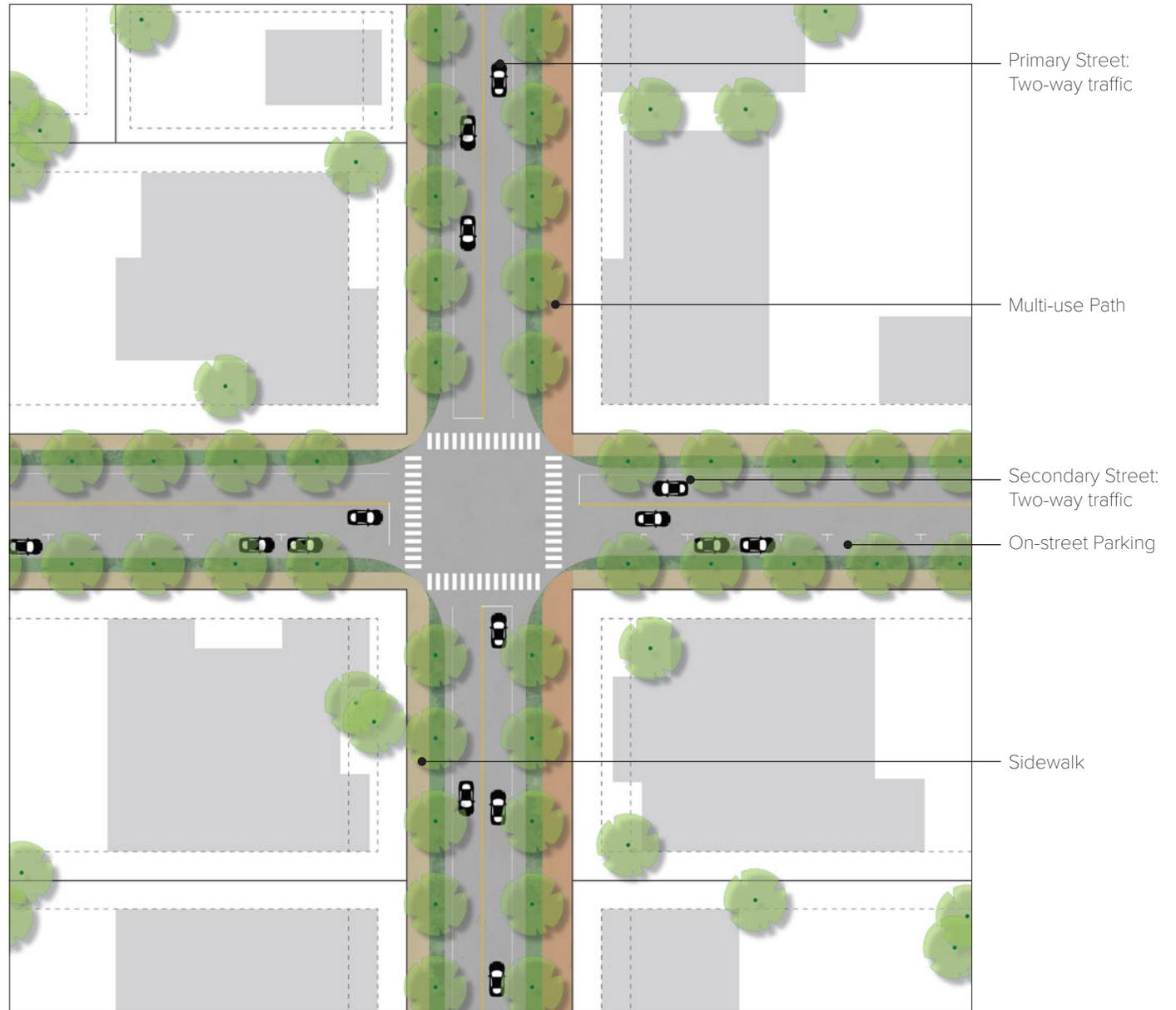


FIGURE 13. DAVIS STRAITS COMMERCIAL CLUSTER EXAMPLE AREA DIAGRAM

Village Mixed Use

Areas with a mix of one to two-story retail, office, and residential development with smaller building footprints and small front yard landscaped setbacks. Buildings are oriented with the narrow face along the street edge to maintain historic development patterns. Behind these frontage buildings, larger structures are possible. The east side of Route 28 between Spring Bars Road/Dillingham Ave and Jones Road/Worcester Court provide model buildings for this type of development. Parking located to the rear, or if 2 spaces or fewer, parking can be located to the side.

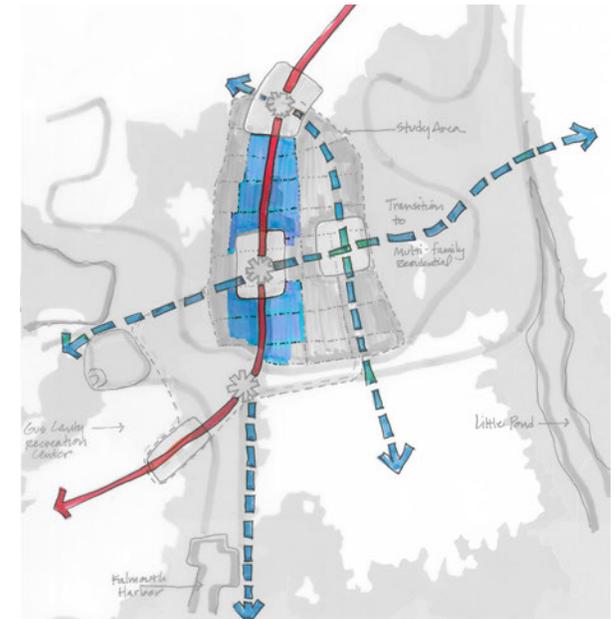
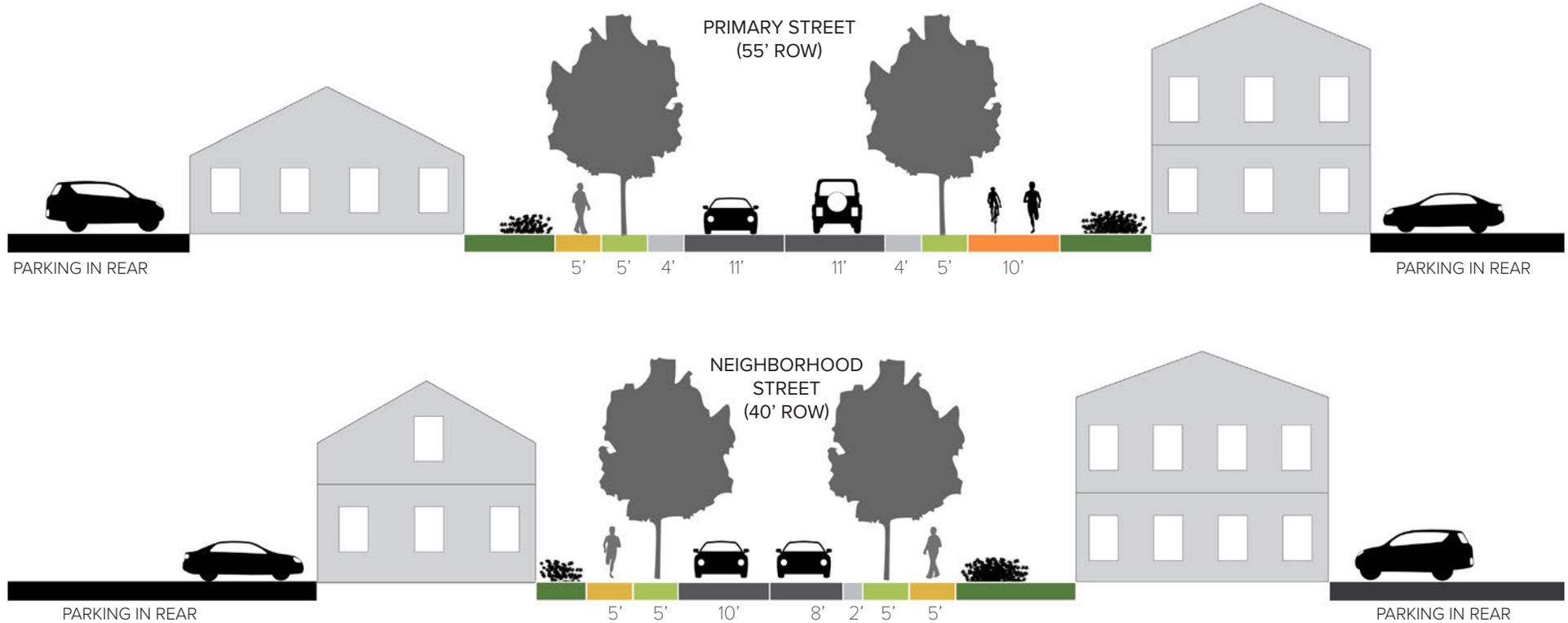


FIGURE 14. EXAMPLE STREET SECTIONS FOR VILLAGE MIXED USE AREAS

STREET SECTIONS

- SIDEWALK
- MULTI-USE PATH
- ON-STREET PARKING
- BUFFER
- SHOULDER
- TRAVEL LANES



Base Residential Density	5 dwelling units per acre
Front Setbacks	5' to 15'
Side and Rear Setbacks	5'
Maximum Building Height	2 stories or 25'
Maximum Building Coverage	20%
Minimum Lot Size (sf)	5,000
Maximum Building Footprint (sf)	2,000
Building Enclosure of Frontage on Principal Street	--
Building Transparency for Street Facing Facades	--
Parking Setback	20'
Parking Placement	Side or rear



FIGURE 15. VILLAGE MIXED USE EXAMPLE AREA DIAGRAM

Residential Mixed Use

Areas with a mix of two-story residential, office development, or home-based businesses (in-home businesses) located close to the street edge or with small front yard setbacks. The area could include small pocket parks with pedestrian amenities. Added green areas reflect greater residential focus, including multi-family development. Parking located on the street or behind buildings in shared courtyard layout. No transparency or street enclosure requirements.

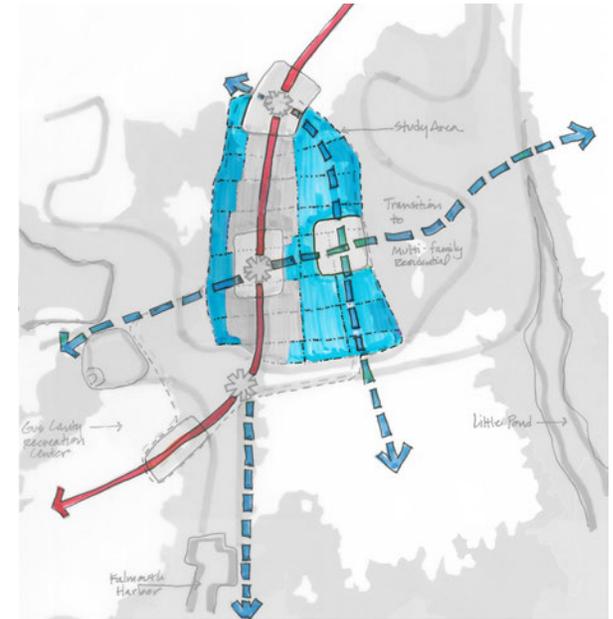
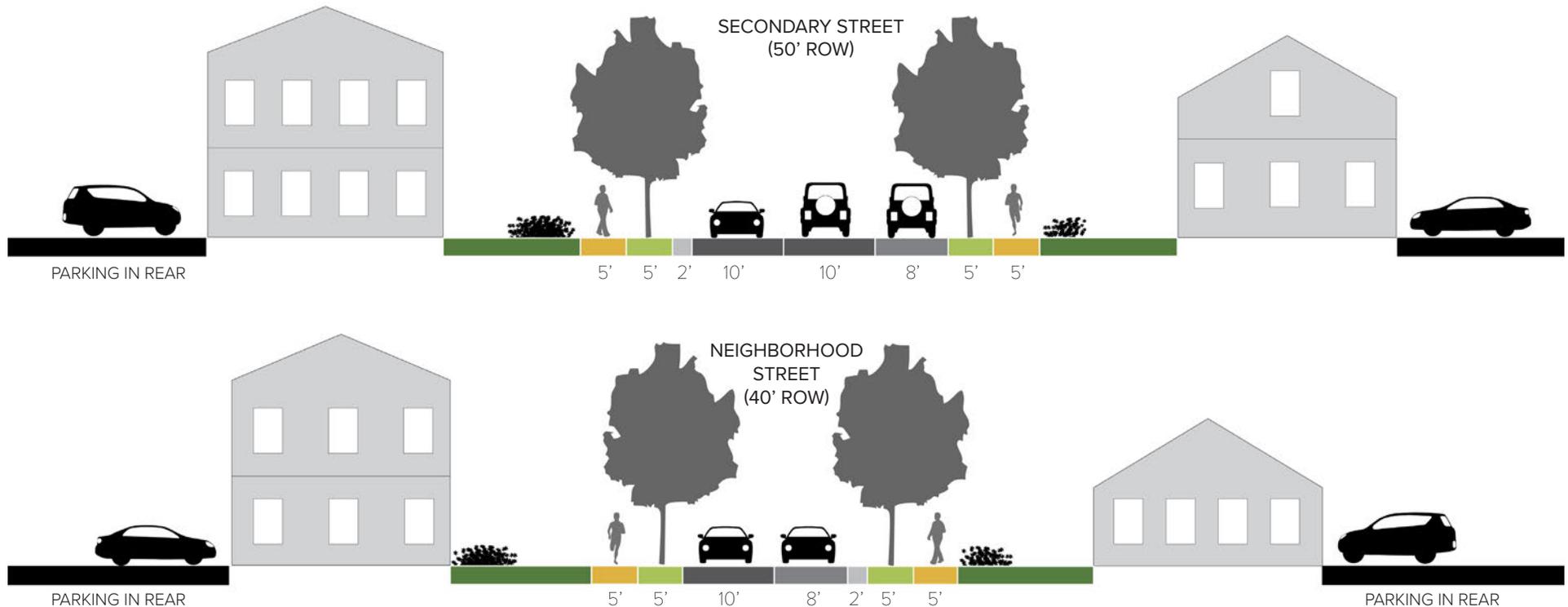


FIGURE 16. EXAMPLE STREET SECTIONS FOR RESIDENTIAL MIXED USE AREAS

STREET SECTIONS

- SIDEWALK
- MULTI-USE PATH
- ON-STREET PARKING
- BUFFER
- SHOULDER
- TRAVEL LANES



Base Residential Density	12 dwelling units per acre
Front Setbacks	15'
Side and Rear Setbacks	10'
Maximum Building Height	2.5 stories or 30'
Maximum Building Coverage	30%
Minimum Lot Size (sf)	10,000
Maximum Building Footprint (sf)	15,000
Building Enclosure of Frontage on Principal Street	--
Building Transparency for Street Facing Facades	--
Parking Setback	20'
Parking Placement	Rear and street

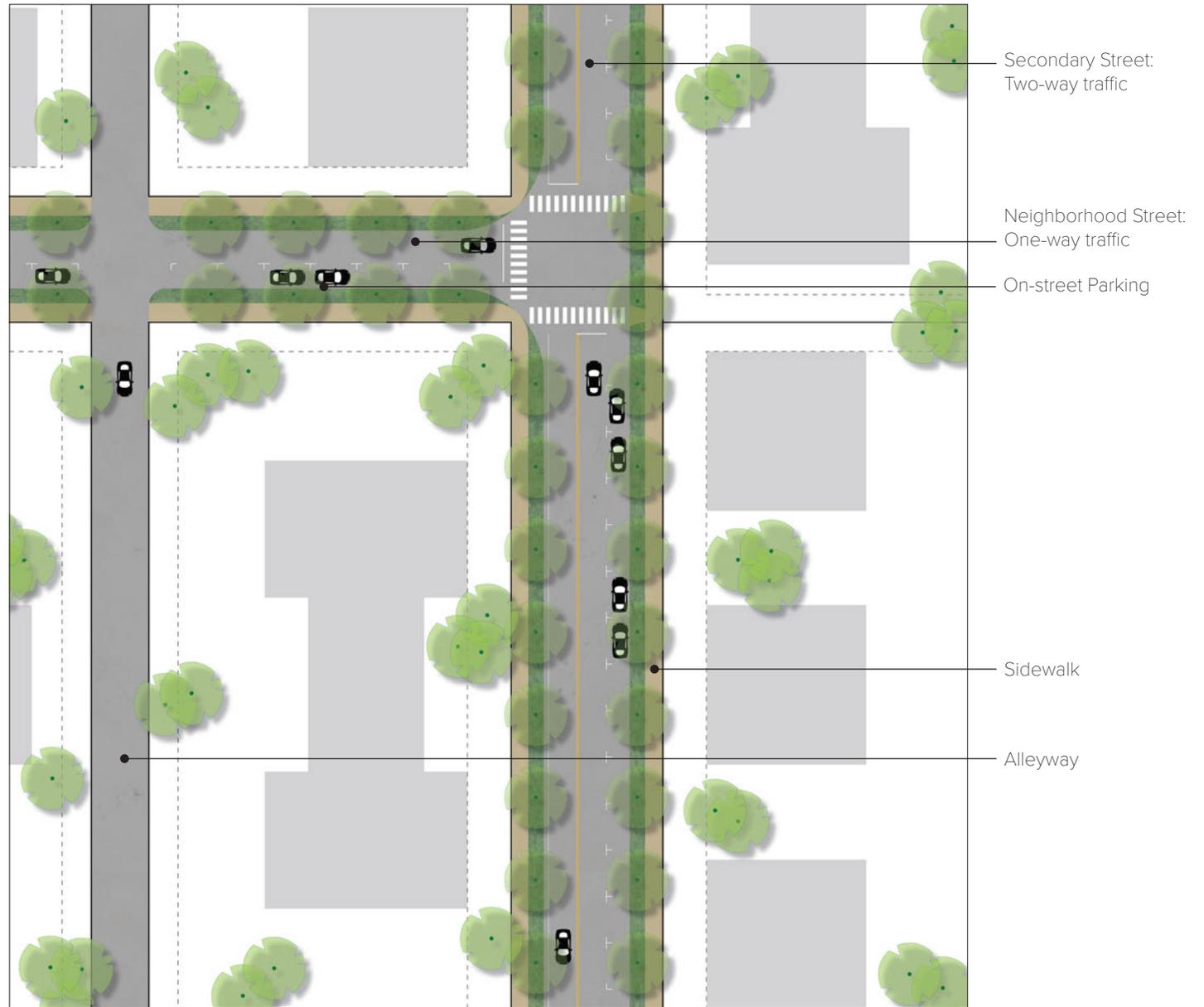


FIGURE 17. RESIDENTIAL MIXED USE EXAMPLE AREA DIAGRAM

Neighborhood Commercial Cluster

Areas adjacent to smaller intersections with neighborhood retail and services to support local needs. Building envelopes are smaller than in Davis Straits Commercial Cluster, but remain close to the street front to reinforce the small scale, pedestrian focus. Parking located on the street or to the side or rear of buildings. Street enclosure and transparency requirements are less than Davis Straits Commercial Clusters. Two small areas are included, at the intersection of Worcester Court/Spring Bars Road and along Route 28 across from the Community Center.

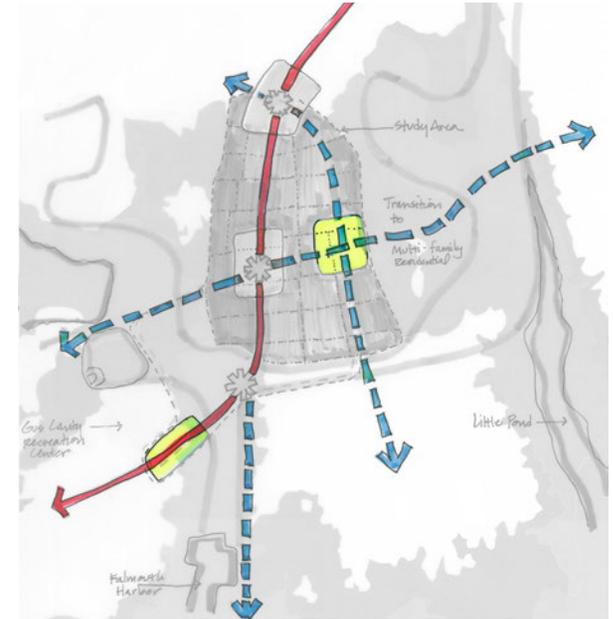


FIGURE 18. EXAMPLE STREET SECTIONS FOR NEIGHBORHOOD COMMERCIAL CLUSTER AREAS

STREET SECTIONS

- SIDEWALK
- MULTI-USE PATH
- ON-STREET PARKING
- BUFFER
- SHOULDER
- TRAVEL LANES



Base Residential Density	5 dwelling units per acre
Front Setbacks	10'
Side and Rear Setbacks	10'
Maximum Building Height	2 stories or 25'
Maximum Building Coverage	30%
Minimum Lot Size (sf)	10,000
Maximum Building Footprint (sf)	5,000
Building Enclosure of Frontage on Principal Street	70% minimum at setback
Building Transparency for Street Facing Facades	40% of ground floor transparent
Parking Setback	20'
Parking Placement	Side or rear

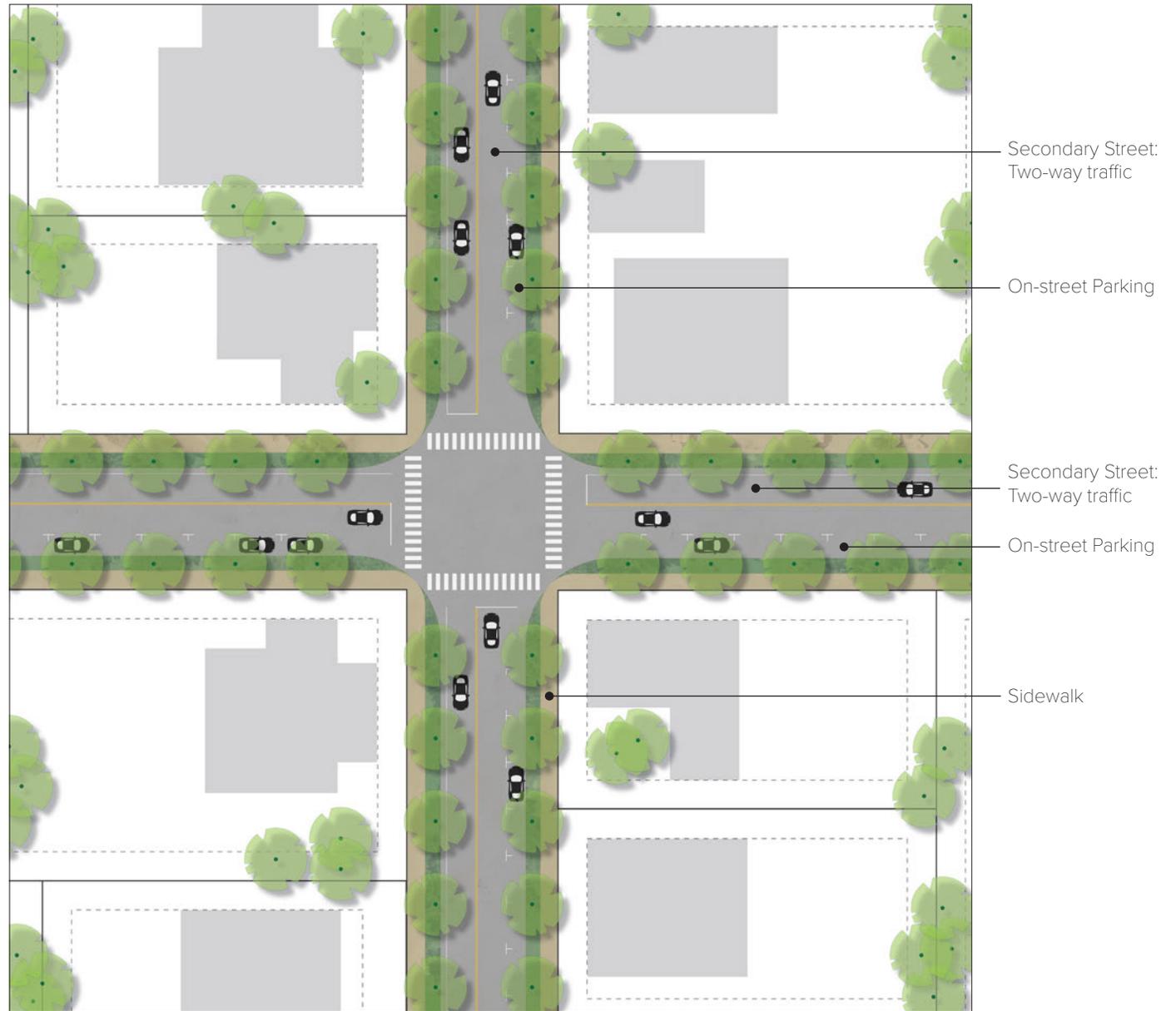


FIGURE 19. NEIGHBORHOOD COMMERCIAL CLUSTER EXAMPLE AREA DIAGRAM

Greenway

Areas that are vulnerable to flooding or that contain existing protected open space or recreation areas. Development limited to active or passive recreational uses and creation of flood storage capacity through removal of development, land acquisition, or easements.

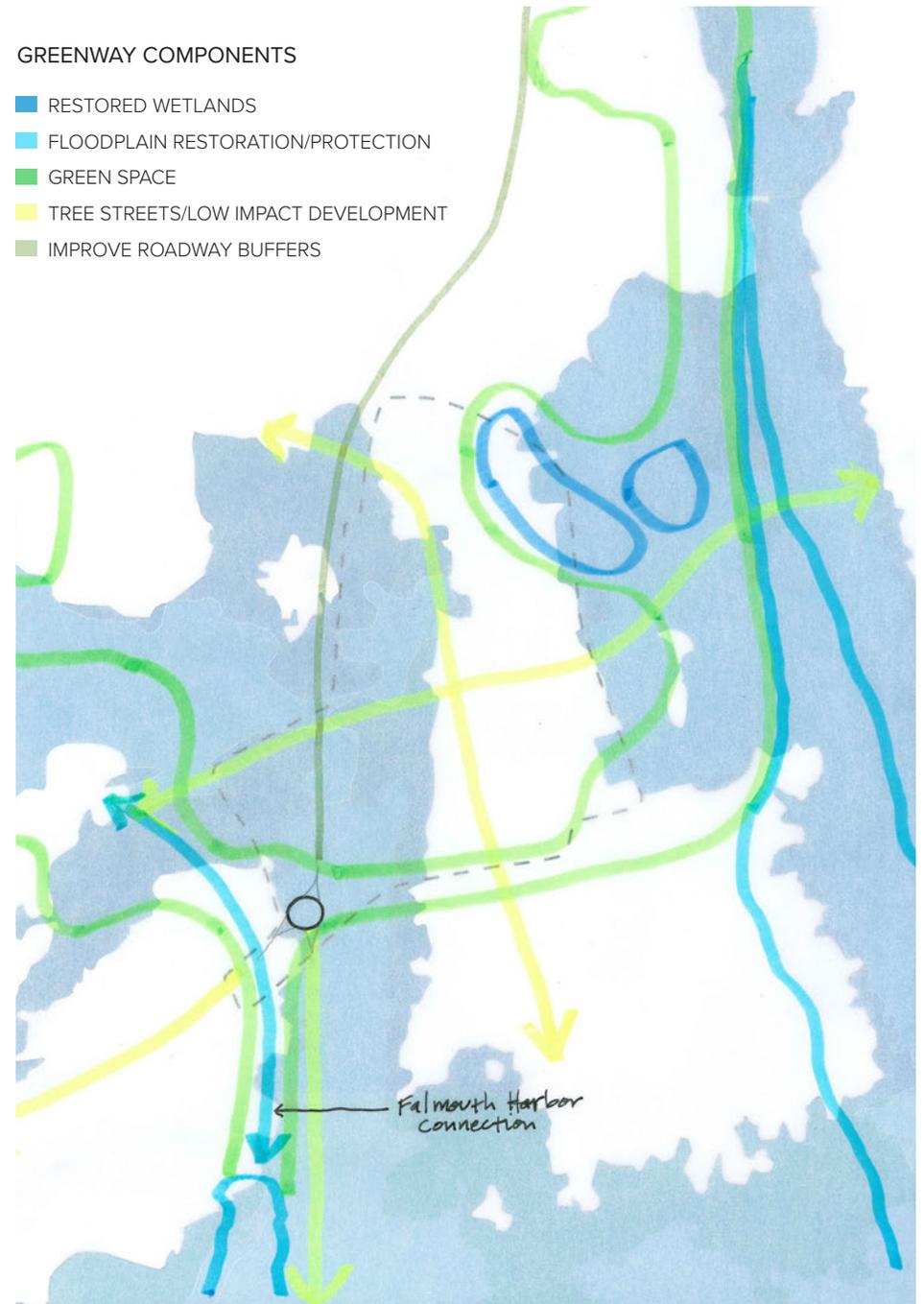


FIGURE 20. GREENWAY CONCEPT SCHEMATIC

HIERARCHY OF STREETS AND WAYS

In addition to the character area descriptions, Commission staff identified a potential hierarchy of streets and ways within the Davis Straits planning area that would assist in defining and creating transitions between Character Areas.

Primary Streets – Route 28

- Streets that carry greater volumes of through traffic
- Roadway improvements focused on safety, turning movements
- Few or consolidated curb cuts
- Turning pockets/lanes where necessary
- Street trees and landscaped or hardscaped front setback
- Sidewalks and multi-use path with grass or landscaped strip between road and walkway
- Parking located behind buildings in shared courtyard layout

Secondary Streets – Worcester Court, Dillingham Road to Spring Bars Road

- Two-way tree-lined streets with LID improvements
- Sidewalks with grass or landscaped strip between road and walkway
- Parking located behind buildings in shared courtyard layout or on the street (ensuring on-street parking does not conflict with planned bike routes on secondary streets)
- Consolidated curb cuts

Neighborhood/Connector Streets

- Narrow, tree-lined streets with LID improvements
- On-street parking or parking to rear
- Sidewalks with grass or landscaped strip between road and walkway
- One-way or two-way streets relieving congestion from major roadways

Prominent Intersections

- Locations with special paving, lighting, or landscape treatments
- Street edge defined by buildings with small front yard setbacks or vistas of open space/parks
- Serve as gateways to other districts

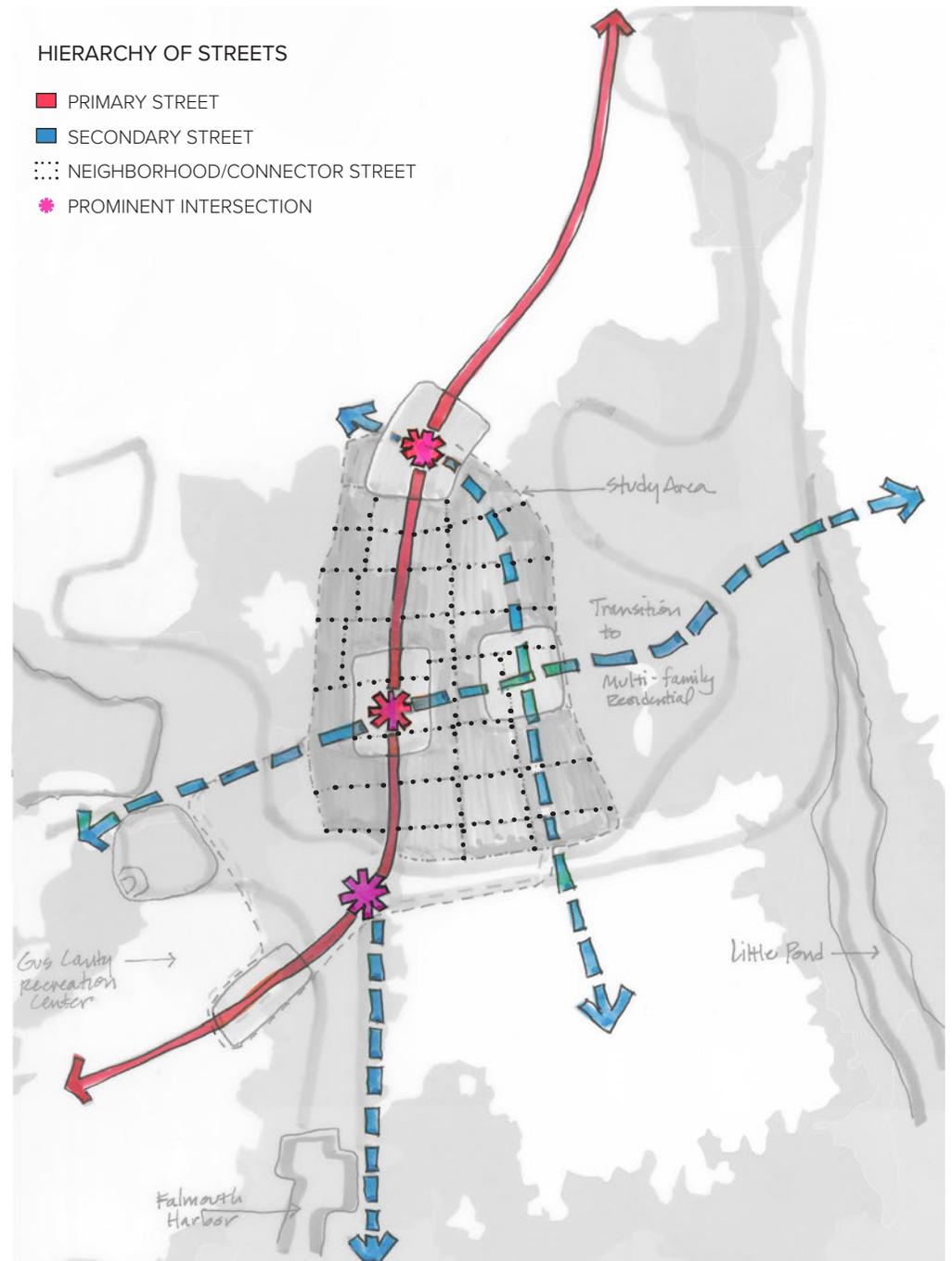
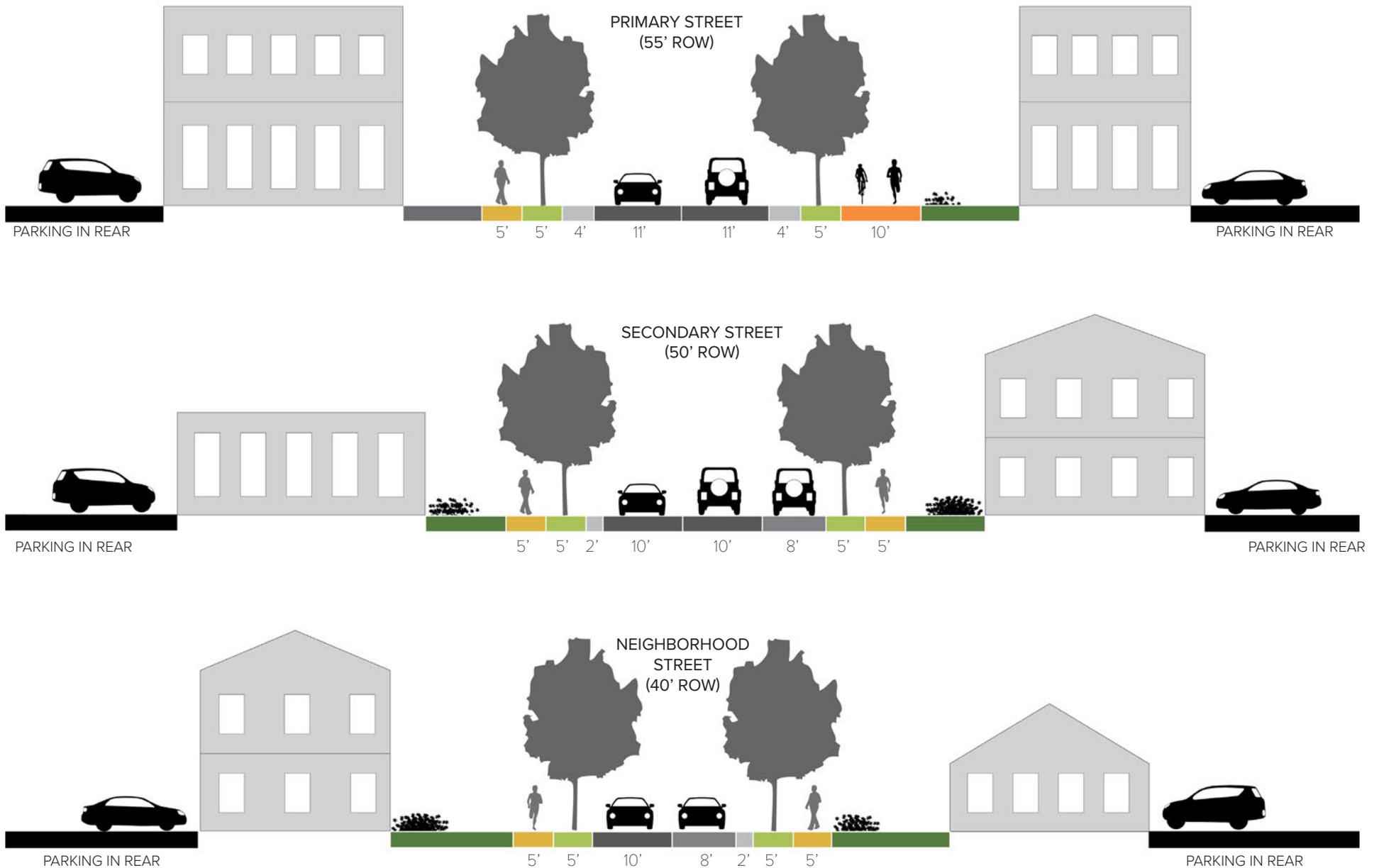


FIGURE 21. EXAMPLE STREET NETWORK

FIGURE 22. STREET SECTION EXAMPLES

■ SIDEWALK
 ■ BUFFER
 ■ SHOULDER
 ■ MULTI-USE PATH
 ■ ON-STREET PARKING
 ■ TRAVEL LANES



CONCEPTUAL GREENWAY PLAN

Following analysis of existing and historic resources within the study area, as well as consideration of the extent of the floodplain and stakeholder comments, we suggest that the town consider establishing greenways in two separate locations within the study area.

As requested in the scope, Commission staff examined opportunities for creating a greenway between Teaticket Park and the Spring Bars Road parcel (Greenway Plan #1). With these two parks or open space areas as anchors, and the Little Pond Brook as a link, Commission staff developed a concept plan for the Town's consideration that would restore habitat and the flood storage capacity within the floodplain, while providing opportunities for passive recreation and pedestrian passage between the two anchor parks. The greenway plan for Little Pond Brook considers relocation of buildings from the lowland to higher elevations on the Falmouth Mall property over the long term, and restoration of the cranberry bogs to functioning wet meadows or bog systems, while maintaining some vehicle access between Spring Bars Road and Maravista Avenue. Stormwater wetlands could also be created on the southeastern corner of the parcel where stormwater could be treated to remove nutrients and also designed to accommodate floodwaters. Conceptually, new buildings within this area would be mixed use

or residential, multistory, and parking could be provided within the floodplain or in parking garages.

Commission staff also suggest an additional greenway area for the town's consideration that would reconnect Morse Pond with Falmouth Inner Harbor. (Greenway Plan #2) The undergrounded stream, which anecdotally is the cause of storm related flooding along Davis Straits, could be daylighted and vegetated buffers restored. Walking or multi-use paths could link facilities such as the ball fields, Gus Canty Community Center, and even the Morse Pond school further to the north, with the Falmouth Inner Harbor park to the south, and other facilities. Over time, the town could acquire properties within the floodplain in this section of the study area and undevelop them, creating opportunities for opening vistas from Route 28 north and south along the restored brook and creating an open field (reminiscent of the area's agricultural past) at the proposed roundabout at Falmouth Heights Road and Route 28. At the same time, the Town could encourage redevelopment and infill development on higher ground in the area between the Gus Canty Community Center and Dillingham Avenue, consistent with the neighborhood vision detailed above.



FIGURE 23. CONCEPTUAL GREENWAY PLAN LOCATIONS

FIGURE 24. GREENWAY PLAN #1: SPRING BARS ROAD TO TEATICKET PARK



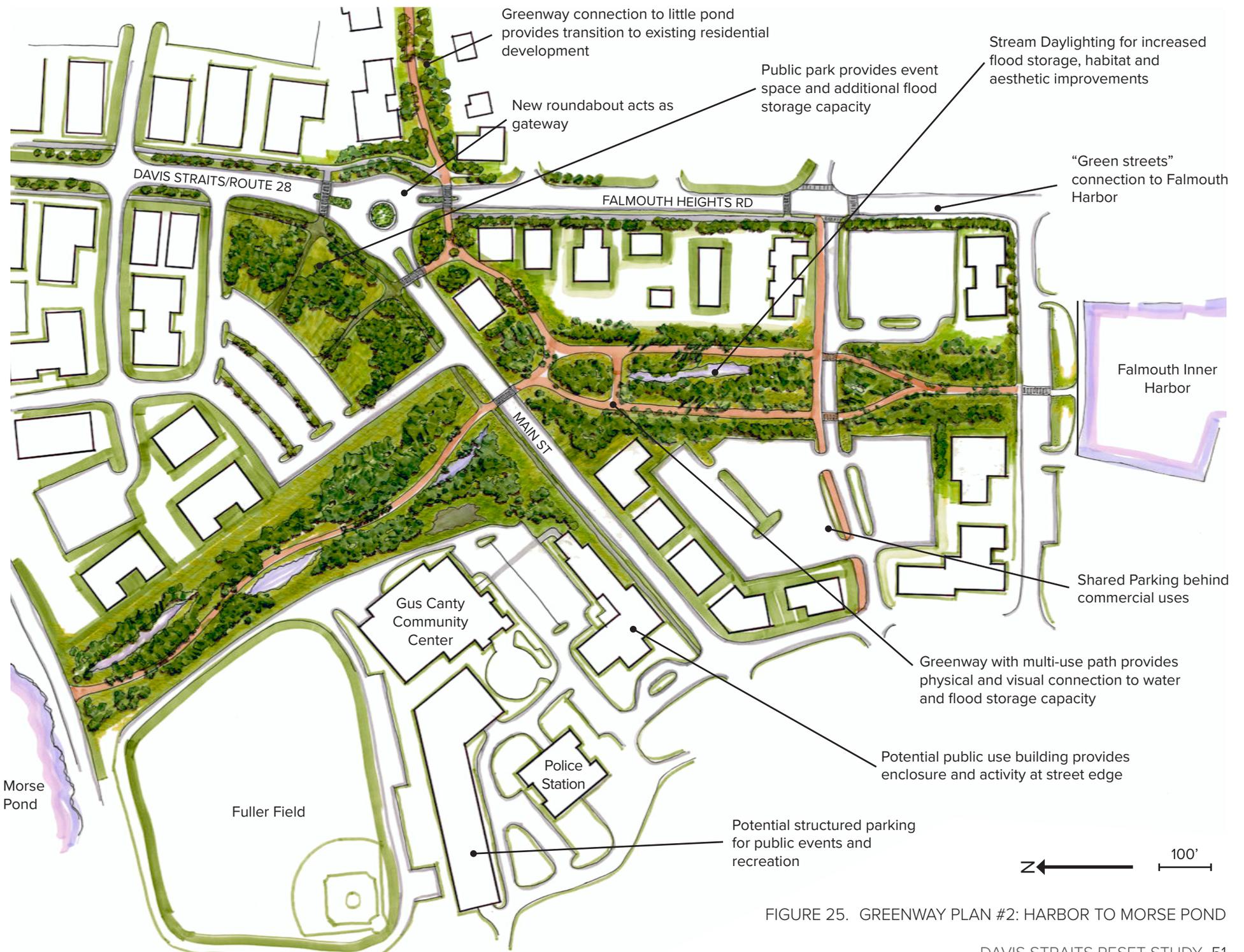


FIGURE 25. GREENWAY PLAN #2: HARBOR TO MORSE POND



NEXT STEPS

In the Spring Bars Road report prepared for the town by the Commission (a precursor to this study), Commission staff made several recommendations to encourage changes in development patterns and form, and to improve the character and function of the area. In summary, the recommendations of that study included encouraging mixed use development as a transition between business and residential areas to the south and east, creating design guidelines to move buildings closer to the street edge and prevent parking in front yards, developing a master plan for the large commercial areas, improving landscaping and pedestrian amenities, and retaining the existing pattern of small buildings with a consistent setback, but allowing more varied uses.

Through the Davis Straits RESET project, Commission staff has examined these and other related issues in more detail and conducted additional outreach to gauge community support for future changes to the area. Based on this analysis, staff offers a series of next steps to assist the town in moving forward with initiatives to support changes in form and character.

These steps are divided into the following categories with short-, medium- and long-term actions:

- Building Form
- Land Use
- Streetscape/Roadway Improvements
- Greenway Plan

BUILDING FORM

Goal: to acknowledge different character areas within the study area, and to create a more pedestrian friendly environment by bringing buildings closer to the road edge and improving pedestrian amenities.

Short-Term

- The Town should consider changes in dimensional regulations to differentiate areas within the Davis Straits study area, including minimum lot sizes and coverage, building setbacks, building heights and massing.
- The Town should consider establishing a build-to line for new development and redevelopment in the study area
- The Town should consider requiring a pedestrian-oriented façade on neighborhood streets within the study area.
- The Town should consider requiring parking to be located to the side or rear of all development within the study area.

Long-Term

- The Town should consider whether form-based code would meet the Town's objectives for the area.
- The Town should consider adding a frontage building to define the streetscape and promote pedestrian activity in front of the Gus Carty Community Center.
- The Town should evaluate the use of Tax Increment Financing (TIF) as an incentive to developers to build according to the Town's vision for the area. This tool releases developers from paying taxes on the increased assessed value generated by redevelopment for a period of time.



LAND USE

Goal: to differentiate the Davis Straits area from other commercial areas in town and to provide more mixed use development and housing opportunities, while improving traffic flow and walkability.

Short-Term

- The Town should consider making desired uses, such as mixed use, by-right in this area and remove undesired uses or shift them to special permit uses to encourage redevelopment that is consistent with the town's vision for the area.
- The Town should consider adopting flood zone building requirements.
- The Town should consider adoption of additional special permit criteria reflective of the vision for this area or sub-areas within it.
- The Town should consider limiting locations for auto service stations within the study area, i.e. not allowed at intersections.

Medium-Term

- Using the conceptual redevelopment plan as a guide, the Town should develop consensus on a proposed vision for the neighborhoods within Davis Straits and create a master plan for adoption by Town Meeting.
- The Town should evaluate the use of form regulations, including maximum building sizes, rather than density limits (i.e., residential units per acre) as a means of protecting community character while allowing developers more flexibility, which in turn could result in more affordable housing options.
- The town should consider adoption of an overlay district for all or part of the study area under which different dimensional regulations could be established, including provisions for parking reductions and payments in lieu of parking.
- The Town should evaluate enhancing the Transfer of Development Rights (TDR) by-law to shift development to higher elevations in Davis Straits and out of areas with sensitive resources and insufficient wastewater treatment or other infrastructure to mitigate impacts to those resources.
- The Town should consider adopting incentives that could be included within the zoning bylaw to encourage desired uses.

STREETSCAPE/ROADWAY IMPROVEMENTS

Goal: to implement safe roadways with pedestrian and bike amenities, reduced visual clutter and curb cuts, and increased tree cover and landscaping.

Short-Term

- The Town should continue to advance the design of a roundabout for the intersection Route 28 at Falmouth Heights Road as proposed in the Transportation Master Plan for Route 28/Main Street prepared by McMahon Associates and recommended by a unanimous vote of the Board Selectmen at their March 12, 2015 joint meeting with the Planning Board.
- The Town should coordinate with the Cape Cod Commission and Massachusetts Department of Transportation to start the project development process (i.e., Project Need Forms) and investigate funding availability for potential improvement along Davis Straits and at the intersection with Dillingham Avenue/Spring Bars Road.
- The Town should consider seeking funding for bicycle racks through the Cape Cod Commission Bicycle Rack Program.
- The Town should consider requiring additional on-site amenities including seating and landscaping through bylaws.
- The Town should consider identifying town-owned roads where street trees may be added. An inventory and evaluation of current conditions would identify needs along suitable roadways.



Medium-Term

- The Town should consider developing consensus through additional outreach on the following alternatives from the Transportation Master Plan for Route 28/Main Street prepared by McMahon Associates:
- Design and streetscape improvements along Davis Straits to provide the framework for further discussion about development patterns along the corridor;
- Long-term improvements for the intersection of Route 28 at Dillingham Avenue/Spring Bars Road to anchor future redevelopment of the area; and
- Long-term improvements for the intersection of Route 28 at Scranton Avenue to improve the connection to the harbor for all road users.
- The Town should investigate the feasibility and cost of creating internal roadways and eliminating curb cuts, as illustrated in the conceptual redevelopment plan.

Long-Term Actions

- The Town should work towards the Falmouth Bikeway Committee's vision of a walkable and bikable town through the implementation of recommended projects in and around the study area as presented in the Town of Falmouth 2015 Bike Plan prepared by the Cape Cod Commission.

GREENWAY PLAN

Goal: To provide flood storage capacity during storms and improve resiliency in the area, as well as to create a recreational amenity linking the harbor with other recreation resources in the study area.

Short-Term

- The Town should pursue adoption of the conceptual greenway plan and identify priorities for acquisition and/or easements, including creating a connection between Falmouth Inner Harbor and Morse pond, and connecting Teaticket Park and the Spring Bars Road parcel along Little Pond Brook.

Medium-Term

- The Town should develop a more detailed greenway plan including restoration and green infrastructure projects to reduce flooding, identification of specific parcels for acquisition or easements, and a time frame for implementation.
- The Town should coordinate with the Falmouth Community Preservation Committee to identify potential properties for acquisition within the proposed greenway corridor, including any properties subject to foreclosure.
- The Town should evaluate the use of District Increment Financing (DIF) to fund greenway development and streetscape improvements. By designating the Davis Straits area along with East Main Street, the Harbor area, and the land around the proposed greenway as a DIF district, the Town could dedicate new property tax revenues generated from redevelopment in the area to the building of the greenway.





APPENDICES

APPENDIX A: HISTORICAL INFORMATION

Massachusetts Historical Commission data:

The Massachusetts Cultural Resource Inventory System (MACRIS) has no inventoried historic properties from the study area, though a series of bungalows along the east side of Route 28 (and some on the west side) date from the early 1900s. A 19th century house and barn adjacent to the CVS property were recently demolished. The Falmouth Poor House (circa 1769) and Methodist Society Burial Ground (circa 1814) are located just west of the study area at 744 Main Street.

Davis Straits Evolution as seen in Historic Maps and Aerial Photos:

Historically, Davis Straits was a less developed area located between clusters of denser development. A series of historic maps and aerial photographs of the study area illustrate many of the changes that occurred.

- 1858 map shows a few houses on the east side of Route 28 between Falmouth Heights Road and Jones Road, while areas to the north and west along Route 28 have many more buildings. This map also shows a stream connecting Bowman Pond (now Falmouth Inner Harbor) with Morse Pond north of the Recreation center complex. While not visible, there must have been a bridge to cross on Route 28.
- 1893 map still has only a few buildings along this stretch of Route 28 (one was house recently demolished for the CVS expansion), and nothing to the east. This map also shows a stream between Bowman Pond and Morse Pond, with a marshy area indicated north of Bowman Pond and south of Route 28. This map reveals that Falmouth Heights development had begun near the ocean waterfront.
- 1938 aerial photo shows farmland and few buildings on both sides of Route 28 near the Falmouth Heights Road intersection, with a wetland area between the harbor and Morse Pond. This photo also shows the row of 10 houses along the east side of Route 28 between Jones Road and Spring Bars Road. Spring Bars Road is laid out and appears to access farm land and cranberry bogs in the area of Worcester Court (no Worcester Court road yet), plus wooded area on much of Spring Bars Road site and to the south. The photo shows the road layout for Falmouth Heights was established through mostly wooded areas north of the coast.
- 1941 USGS map confirms what's shown in the 1938 photo, but with a few houses added along the northern range of Falmouth Heights. Falmouth Inner Harbor is named on this map.
- 1952 aerial photo shows continued farmland (and farmhouse in center) to the east of Route 28 between Falmouth Heights and Spring Bars Road, plus

behind the bungalows on Route 28. More wooded areas are shown to the west of Route 28, and a baseball field visible south of Morse Pond. Large buildings are on the north side of Route 28 below the ball field. Many more houses are visible in the northwestern quarter of Falmouth Heights, but the northeastern quarter is still all wooded.

- 1971 aerial photo shows what had been a large farmstead transformed into a large mall and parking area, and smaller farmed fields across the street have become smaller buildings with parking lots. Cranberry bogs are still visible off Spring Bars Road, but a large development site is appearing north of them. Worcester Court is built (likely following what had been a dirt track between earlier farm fields) and some additional new development is visible on the east side behind the new mall. The wetland connection between the harbor and the pond is paved over both north and south of Route 28, and large buildings face the harbor.
- 1984 aerial photo shows cranberry bogs replaced by the Falmouth Mall.
- 1994 aerial photo shows an expanded Falmouth Mall and new commercial development on the east side of Worcester Court. This photo also shows the new Recreation Center set back from Route 28 and the earlier building removed from the street edge.

APPENDIX B: ECONOMIC DEVELOPMENT INFORMATION

Analytical Methodology

This section provides a brief overview of how the Davis Straits area was analyzed from an economic perspective.

Research Questions

- Research question help to define the purpose or goal of research and analysis. For this project, the research questions were:
- What types of businesses are in the Davis Straits study area and how many people do they employ? How does this compare to the Village Center?
- What share of all the jobs in Falmouth as well as all Falmouth business, sales, real-estate sales, and AV come from the Davis Straits study area?
- What is the value of Davis Straits businesses within different industry sectors to the Town in terms of job quality, export sales, and assessed value relative to each other and to similar businesses town wide?
- What is the value of these businesses to the Town in terms of job quality, export sales, and assessed value relative to the Village Center?
- The value of the residential uses as measured by real-estate sales and AV relative to commercial uses and by different residential types?

Data Analysis

To understand Davis Straits from an economic perspective, it is necessary to compare the area to other areas within the Town of Falmouth. In this study, we compared business activity and land use types in Davis Straits to the core of the Village Center and to the town as a whole. It is also important to understand the value of business and land uses relative to each other. This study, therefore, compares business across industries and land uses by major use type (commercial, residential, and mixed use) and by sub-groupings within these use types. This requires “normalizing” the comparison variables such as employment, sales, assessed value and the like by dividing them by a common variable such as lot size or business square footage for example.

Data Sources

The breadth of this analysis of business activity and land uses in Davis Straits by industry type and relative to the rest of the town and the Village Center is determined by the data available. This study relied on three datasets:

1. Business Analyst, ESRI
2. Quarterly Employment and Wages (QEW or ES202), US Bureau of Labor Statistics
3. Property Tax Assessors Data, Town of Falmouth

Business Analyst provides data on individual businesses that is collected from a variety of sources. The dataset includes the location and name of the business, the number of employees, and estimate of sales, and the amount of space the business occupies in square feet. Most of the businesses included in the dataset are classified into industry sectors using the Federal NAICS codes. Sales data is imputed from national sales levels by industry type rather than actual sales for each of the actual businesses and does not factor in the seasonality of the local market.

The Business Analyst data on employment does not specify whether the employee work full or part-time and the dataset does not provide wage data. The wage data used in this report comes from the Quarterly Employment and Wages dataset for the Town of Falmouth collected as part of the Unemployment Insurance system and ultimately compiled by the US Bureau of Labor Statistics.

The Property Tax Assessors Data provides detailed information on land use and buildings by individual property. This data is used to determine the taxable value (“assessed value”) of each property by considering numerous variables including lot size, building size and use, building structural characteristics, and location. The assessed value must also reflect market prices for the sale of similar properties in similar locations. The latest sale price is included as a variable for each property in Falmouth.

TABLE B1. DAVIS STRAITS STUDY AREA – BUSINESSES BY INDUSTRY SECTOR

Industry Sector - Individual	# of Businesses	Estimated Employment	Total SF
Grand Total*	133	841	468,355
Retail Trade	27	253	137,711
Accommodation and Food Services	20	173	70,909
Professional, Scientific, and Technical Services	15	51	32,769
Other Services (except Public Administration)	14	39	39,965
Finance and Insurance	9	38	29,986
Administrative and Support and Waste Management	8	46	16,759
Real Estate and Rental and Leasing	8	43	26,738
Health Care and Social Assistance	7	30	21,103
Construction	5	26	18,208
Transportation and Warehousing	4	25	16,183
Management of Companies and Enterprises	3	6	6,702
Manufacturing	3	9	7,785
Wholesale Trade	3	14	12,329
Arts, Entertainment, and Recreation	2	3	3,266
Educational Services	2	0	0
Public Administration	2	83	25,610
Information	1	2	2,332

* There were an additional 19 Unclassified Businesses not included in this table
 Source: ESRI Business Analyst

TABLE B2. DAVIS STRAITS STUDY AREA – BUSINESSES BY INDUSTRY (SORTED BY ADDRESS)

BUSINESS NAME	STREET ADDRESS	INDUSTRY SECTOR CLASSIFICATION
PAUL'S PIZZERIA	14 Benham Rd	Accommodation and Food Services
FRIENDLY'S	5 Davis Straits	Accommodation and Food Services
AMAZING NAILS	7 Davis Straits	Other Services (except Public Administration)
OLYMPIA SPORTS	7 Davis Straits	Retail Trade
STAPLES	7 Davis Straits	Retail Trade
DOLLAR TREE	7 Davis Straits	Retail Trade
STARBUCKS	11 Davis Straits	Accommodation and Food Services
HEADS UP BEAUTY DESIGNS LTD	15 Davis Straits	Other Services (except Public Administration)
CAMBRIDGE EYE	17 Davis Straits	Retail Trade
ROCKLAND TRUST COMPANY	20 Davis Straits	Finance and Insurance
PIER 1	23 Davis Straits	Retail Trade
NEW DYNASTY RESTAURANT	25 Davis Straits	Accommodation and Food Services
DYNASTY BUFFET	28 Davis Straits St	Accommodation and Food Services
AMCOMM WIRELESS INC	29 Davis Straits	Wholesale Trade
NINETY NINE RESTAURANT & PUB	30 DAVIS STRAITS	Accommodation and Food Services
BOOKSMITH	33 Davis Straits	Retail Trade
PETCO	35 Davis Straits	Retail Trade

TABLE B2. DAVIS STRAITS STUDY AREA – BUSINESSES BY INDUSTRY (SORTED BY ADDRESS)

BUSINESS NAME	STREET ADDRESS	INDUSTRY SECTOR CLASSIFICATION
U HAUL	40 Davis Straits	Real Estate and Rental and Leasing
STOP AND SHOP GAS STATION	43 Davis Straits	Retail Trade
CITIZENS BANK	50 Davis Straits	Finance and Insurance
MATTRESS WORLD, INC.	56 Davis Straits Unit 1	Retail Trade
PAPA GINO'S	56 Davis Straits Unit 2	Accommodation and Food Services
REAL ESTATE ASSOCIATES	59 Davis Straits	Real Estate and Rental and Leasing
A & M MVG CORP.	59 Davis Straits	Not classified
CENTURY 21	59 Davis Straits	Real Estate and Rental and Leasing
EAGLE PAINTING INCORPORATED	63 Davis Straits	Construction
AT T	64 Davis Straits	Manufacturing
AUTOZONE	64 Davis Straits	Retail Trade
TOMINDE, L. L. P.	69 Davis Straits	Not classified
THREE OF A KIND, INC.	70 Davis Straits	Professional, Scientific, and Technical Services
C L S LLC	70 Davis Straits	Not classified
WILLIAM SCHUTTEN MD	72 Davis Straits	Health Care and Social Assistance
MIRACLE EAR	72 Davis Straits	Retail Trade
CHDAMA CORPORATION	75 Davis Straits	Not classified
C SALT BAR & GRILLE	75 Davis Straits	Accommodation and Food Services

TABLE B2. DAVIS STRAITS STUDY AREA – BUSINESSES BY INDUSTRY (SORTED BY ADDRESS)

BUSINESS NAME	STREET ADDRESS	INDUSTRY SECTOR CLASSIFICATION
MEZZA RESTAURANT	75 Davis Straits	Accommodation and Food Services
ROCKYMAN LLC	75 Davis Straits	Not classified
EAT YOUR HEART OUT CATERING COMPANY	79 Davis Straits R	Accommodation and Food Services
ALL CAPE POWER & CYCLE INC	79 Davis Straits	Not classified
CAPE OPTICIANS	87 Davis Straits	Retail Trade
CLOVER LANDSCAPE & MONUMENT	90 Davis Straits	Administrative and Support and Waste Management and Remediation Services
MCDONALD BROTHERS MONUMENT CO	90 Davis Straits	Retail Trade
VAGABONDVIEW PHOTOGRAPHY	95 Davis Straits	Professional, Scientific, and Technical Services
SLEEPYS FALMOUTH	96 Davis Straits	Retail Trade
VON HAAM KAREN	99 Davis Straits	Health Care and Social Assistance
K K CODDERS	100 Davis Straits	Accommodation and Food Services
CVS	105 Davis Straits	Retail Trade
CVS	105 Davis Straits	Retail Trade
FALMOUTH PET CENTER	112 Davis Straits	Retail Trade
ISLAND QUEEN	297 Dillingham Ave	Transportation and Warehousing
COMMONWEALTH TRAVEL INC	299 Dillingham Ave	Administrative and Support and Waste Management and Remediation Services

TABLE B2. DAVIS STRAITS STUDY AREA – BUSINESSES BY INDUSTRY (SORTED BY ADDRESS)

BUSINESS NAME	STREET ADDRESS	INDUSTRY SECTOR CLASSIFICATION
SHAW CHIROPRACTIC	301 Dillingham Ave	Health Care and Social Assistance
OFF SHORE INNOVATIONS	303 Dillingham Ave	Health Care and Social Assistance
UPTOWN BODY	305 Dillingham Ave	Other Services (except Public Administration)
CORCORAN R B CO OF HYANNIS	343 Dillingham Ave	Retail Trade
KISTIN BABITSKY AND BEITMAN ATTORNEYS AT LAW	13 Falmouth Heights Rd	Professional, Scientific, and Technical Services
ABACUS GROUP, INC.	13 Falmouth Heights Rd # 200	Wholesale Trade
HENDERSON HOLDING LTD LIABILIT	13 Falmouth Heights Rd	Management of Companies and Enterprises
OPTIMIZED COMMUNICATIONS CORP.	13 Falmouth Heights Rd	Information
F. ROBERT DEFRANCISCO, INC.	13 Falmouth Heights Rd	Not classified
THE KEEP	13 Falmouth Heights Rd	Not classified
NELSON COMMUNICATIONS, INC.	13 Falmouth Heights Rd # 200	Professional, Scientific, and Technical Services
FALMOUTH BAND PARENTS INC	19 Falmouth Heights Rd	Other Services (except Public Administration)
FALMOUTH MASONIC TRUST	19 Falmouth Heights Rd	Other Services (except Public Administration)
PAYDAY	19 Falmouth Heights Rd	Professional, Scientific, and Technical Services
PRUDENTIAL	23 Falmouth Heights Rd	Finance and Insurance
WATERBURY & OLSON OPTOMETRY PC	27 Falmouth Heights Rd	Health Care and Social Assistance
DFM INSURANCE AGENCY INC	33 Falmouth Heights Rd	Finance and Insurance

TABLE B2. DAVIS STRAITS STUDY AREA – BUSINESSES BY INDUSTRY (SORTED BY ADDRESS)

BUSINESS NAME	STREET ADDRESS	INDUSTRY SECTOR CLASSIFICATION
FALMOUTH POLICE DEPARTMENT	750 Main St	Public Administration
PMJ GOURMET DELI INC	781 Main St	Retail Trade
BOX LUNCH DELI SANDWICHES	781 Main St	Accommodation and Food Services
BEE GEE & CO INC	781 Main St	Not classified
MICHAEL A BULLARD CONCIERGE SERVICES INC DBA DOGGZ & HOGGZ	781 Main St	Not classified
FALMOUTH RECREATION DEPARTMENT	790 Main St	Public Administration
CUMBERLAND FARMS 2342	797 Main St	Retail Trade
MAYTAG SELF SERVICE LAUNDRY	807 Main St	Other Services (except Public Administration)
TOWN LAUNDRY	807 Main St	Other Services (except Public Administration)
BANGKOK THAI CUISINE	809 Main St	Accommodation and Food Services
VILLAGGIO ITALIAN RISTORANTE	824 Main St	Accommodation and Food Services
FALMOUTH INN	824 Main St	Accommodation and Food Services
MARY ELLEN'S PORTUGUESE BAKERY	829 Main St	Manufacturing
DAIRY QUEEN	839 Main St	Accommodation and Food Services
FALMOUTH SHOE REPAIR	845 Main St	Other Services (except Public Administration)
DONAHUE REAL ESTATE	850 Main St Unit 6	Real Estate and Rental and Leasing

TABLE B2. DAVIS STRAITS STUDY AREA – BUSINESSES BY INDUSTRY (SORTED BY ADDRESS)

BUSINESS NAME	STREET ADDRESS	INDUSTRY SECTOR CLASSIFICATION
RR LANDSCAPE & CONSTRUCTION	850 Main St Unit B	Transportation and Warehousing
ALVES CLEANING	850 Main St Unit A	Administrative and Support and Waste Management and Remediation Services
ORLEANS AUTO SUPPLY, INC.	850 Main St Unit 1	Wholesale Trade
FOR BIRDS ONLY INC	850 Main St Unit 6	Retail Trade
VOWS	850 Main St Unit 5	Not classified
EXPRESS TAX SERVICE-FALMOUTH	850 Main St	Professional, Scientific, and Technical Services
THE UPS STORE	850 Main St Unit 3	Administrative and Support and Waste Management and Remediation Services
DJ'S WINGS 'N' THINGS	872 Main St	Management of Companies and Enterprises
BRETHREN, INC.	874 Main St	Other Services (except Public Administration)
DJ S FAMOUS WINGS	874 Main St	Not classified
IDEAL FLOOR COVERING, INC.	882 Main St	Retail Trade
SLICE OF ITALY	890 Main St	Accommodation and Food Services
ITALIAN GOURMET FOODS	890 Main St	Not classified
NEW BEGINNINGS OF CAPE COD	894 Main St	Other Services (except Public Administration)

TABLE B2. DAVIS STRAITS STUDY AREA – BUSINESSES BY INDUSTRY (SORTED BY ADDRESS)

BUSINESS NAME	STREET ADDRESS	INDUSTRY SECTOR CLASSIFICATION
SEASIDE VACATION	26 Robbins Rd	Administrative and Support and Waste Management and Remediation Services
HARBORWALK RESORT CONDOMINIUM ASSOCIATION	26 Robbins Rd	Not classified
RESERVATION OFFICE	26 Robbins Rd	Accommodation and Food Services
BENNETT PLUMBING AND HEATING INC	10 Rose Morin Ln	Construction
FRANK ROSE	22 Rose St	Administrative and Support and Waste Management and Remediation Services
KRISHNA ONE INC	24 Scranton Ave	Accommodation and Food Services
FLAGSHIP MOTEL	24 Scranton Ave	Accommodation and Food Services
FALMOUTH SELF SERVICE LAUNDRY	32 Scranton Ave	Other Services (except Public Administration)
KING S GRANT RACQUET CLUB, INC.	44 Scranton Ave	Arts, Entertainment, and Recreation
DION PAUL ASSOCIATES INC	44 Scranton Ave	Not classified
ANDERSEN, CAROLYN E	44 Scranton Ave	Professional, Scientific, and Technical Services
FABRIC CORNER, INC	12 Spring Bars Rd	Retail Trade
EVERETT MANAGEMENT	19 Spring Bars Rd	Administrative and Support and Waste Management and Remediation Services

TABLE B2. DAVIS STRAITS STUDY AREA – BUSINESSES BY INDUSTRY (SORTED BY ADDRESS)

BUSINESS NAME	STREET ADDRESS	INDUSTRY SECTOR CLASSIFICATION
FALMOUTH LIQUOR MART	21 Spring Bars Rd	Retail Trade
CLANCY APPRAISAL CO INC	24 Spring Bars Rd Ste 3b	Real Estate and Rental and Leasing
LANDUCCI OFFICE MA	24 Spring Bars Rd	Administrative and Support and Waste Management and Remediation Services
FITNESS DIRECTIONS	24 Spring Bars Rd	Arts, Entertainment, and Recreation
RICHARD HUGHES DBA EXCELLENCE IN SAFETY	24 Spring Bars Rd	Educational Services
RONALD TRACY	150 Worcester CT	Health Care and Social Assistance
FALMOUTH AUTO WORKS, INC.	151 Worcester CT	Other Services (except Public Administration)
REAL DEAL AUTO SALES	157 Worcester CT	Retail Trade
ENTERPRISE RENT A CAR	159 Worcester CT	Real Estate and Rental and Leasing
VIKING LOGISTICS CO	161 Worcester CT	Transportation and Warehousing
NAPA	163 Worcester CT	Retail Trade
CLEMENTS AUTO SERVICE	179 Worcester CT	Other Services (except Public Administration)
NORTHEAST INSURANCE AGENCY	194 Worcester CT	Finance and Insurance
SERVICE CREDIT UNION	199 Worcester CT	Finance and Insurance
GRASTON TECHNIQUE	200 Worcester CT APT B	Professional, Scientific, and Technical Services
BILL BLACK AGENCY INC	205 Worcester CT	Transportation and Warehousing

TABLE B2. DAVIS STRAITS STUDY AREA – BUSINESSES BY INDUSTRY (SORTED BY ADDRESS)

BUSINESS NAME	STREET ADDRESS	INDUSTRY SECTOR CLASSIFICATION
GERALD M ROAN DMD	205 Worcester CT Ste 2	Health Care and Social Assistance
GARY DELLAPOSTA CPA	205 Worcester CT Ste 1b	Professional, Scientific, and Technical Services
VIVO BENE, LLC	205 Worcester CT	Not classified
OLDE HARBOR ESTATES HOMEOWNERS	205 Worcester CT Ste 1b	Other Services (except Public Administration)
LANGUAGE LINKS	205 Worcester CT	Educational Services
AIKEN & AIKEN PC	205 Worcester CT	Professional, Scientific, and Technical Services
BRETT A SANIDAS	205 Worcester CT Ste C1	Professional, Scientific, and Technical Services
CREW MICHAEL J REGISTERED INVESTMENT ADVISOR	205 Worcester CT Ste 3	Finance and Insurance
ADVANCE BUSINESS SOLUTIONS	205 Worcester CT	Not classified
ODYSSEY CORPORATION	205 Worcester CT	Not classified
RICHARD RUSSELL	205 Worcester CT Ste 2b	Professional, Scientific, and Technical Services
ECDI HOLDINGS, INC.	205 Worcester CT	Management of Companies and Enterprises
KEEGAN'S CAPE	205 Worcester CT Ste 7b	Retail Trade
FALMOUTH PRINTING & COPY CENTER, INC.	205 Worcester CT Ste 6	Manufacturing
KELLEY LAW, LLC	205 Worcester CT Ste A1	Professional, Scientific, and Technical Services
SOPRANO, JULIANE, LAW OFFICES OF	205 Worcester CT Ste A1	Professional, Scientific, and Technical Services

TABLE B2. DAVIS STRAITS STUDY AREA – BUSINESSES BY INDUSTRY (SORTED BY ADDRESS)

BUSINESS NAME	STREET ADDRESS	INDUSTRY SECTOR CLASSIFICATION
V & V CONSTRUCTION CO INC	205 Worcester CT Ste 8a	Construction
GROUND PENETRATING CARBON, INC.	205 Worcester CT Ste A4	Not classified
POLI MORTGAGE GROUP	205 Worcester CT	Finance and Insurance
LEONARD ESKENAS ATTORNEY	205 Worcester CT	Professional, Scientific, and Technical Services
RE/MAX	205 Worcester CT Ste 4b	Real Estate and Rental and Leasing
KITCHEN KORNER	205 Worcester CT Ste 7b	Construction
ERIE CHEMICAL DISTRIBUTORS	205 Worcester CT Ste C4	Finance and Insurance
RE/MAX	205 Worcester CT Ste 4b	Real Estate and Rental and Leasing
NANTUCKET ALARM CO	205 Worcester CT Ste 8a	Construction

Source: ESRI Business Analyst

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